



ARCHITECT'S URBAN DESIGN STATEMENT
MOYGADDY CASTLE Phase 1A, CO. MEATH An Bord Pleanála - STAGE 3 SUBMISSION
SEPTEMBER 2022

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<i>NEIGHBOURHOOD</i>		
<i>SITE</i>		
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0 EXECUTIVE SUMMARY

This design statement has been prepared in support of a Strategic Housing Development (SHD) application that will be submitted by Sky Castle Ltd. for a Residential and Crèche development at Moygaddy, Maynooth, Co. Meath. As outlined in the Meath County Development Plan DM OBJ 13, this statement provides details of the site analysis, its compliance with national and local planning regulations, including the concept/masterplan for both this site and the wider landholding held (Moygaddy Masterplan), and the urban design rationale for the envisaged residential community, aligning with the 12 criteria of the *Urban Design manual, a Best Practice Guide, Department of Environment, Heritage and Local Government, 2009*.

The 17.52 ha subject site is located 2km north of Maynooth town and forms a key component of the Masterplan for the development of the Maynooth Environs LAP area. The proposed residential scheme is in compliance with the Maynooth LAP as now incorporated within the Meath County Development Plan (CDP), 2021-2027.

Moygaddy Castle residential development will address the current local demand for private family homes with back gardens as well as offering high quality duplex and apartment options. This development aims to generate a new community with a distinct sense of place within a liveable, healthy, diverse and inclusive neighbourhood. Strong identifiable character areas will be developed and integrated into the larger mixed use framework planned at this location. The scheme design has been developed with full regard to the Meath CDP (2021-2027), and it is an integral part of a masterplan framework for all of the multi-use zoned areas of Moygaddy that has been prepared on the lands of the former Maynooth Environs LAP (2009-2015), which has now been incorporated into the aforementioned CDP.

This document outlines the design rationale behind Phase 1A of the Moygaddy Castle development, which comprises a 360-unit residential scheme. A second phase 1B is envisaged in the future for 140no. homes, and together will deliver up to 500 homes as set out in the Core Strategy Housing Allocation of the Meath County Council Development Plan (2021-2027). In addition to the provision of new housing, the scheme envisages the creation of a dynamic network of landscaped parkland within and adjacent to the residential lands (5.8 hectares overall), and a crèche to satisfy childcare needs for this and future developments in the area.

The proposal also includes for the provision of a new bridge over the Rye river as part of the delivery of a section of the Maynooth Outer Orbital Route (MOOR), the realignment of the L-6219 local road for traffic safety, the construction of a new pedestrian/cycle bridge over the Blackhall Little, a Public Park of 2.85ha, a Public Playground, a Scout Den and a Crèche to accommodate 61 children, with potential extension for the broader masterplan community, in addition to pedestrian and cycling links and associated infrastructure.



1 INTRODUCTION

PROJECT STATISTICS

Overall Site Area	19.5223 ha	48.24 acres	195,223 sq.m
Developable Site Area	7.89 ha		
Net Density	45.6 units per hectare	360 homes	
Plot Ratio (Net)	0.40		
Plot Ratio (Gross)	0.18		
Site Coverage (Gross)	10%		

PUBLIC FACILITIES

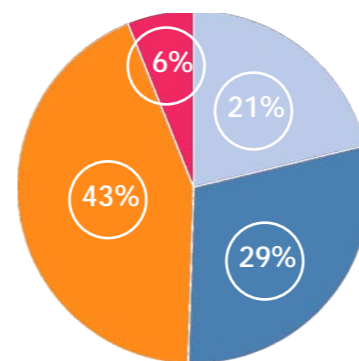
Creche	514 m2
Scout Den	220 m2

OVERALL PUBLIC OPEN SPACE PROVIDED

Public Open Space within zoned lands	0.99 ha 12.6% <i>of residential zoned land</i>	41 % <i>of application gross site area</i>
High Amenity Lands, including Public Park	6.99 ha 36% <i>of application gross area</i>	
Overall total	7.92 ha	

RESIDENTIAL DEVELOPMENT STATISTICS

Dwelling typology	Totals	1-bed	2-bed	3-bed	4-bed
Houses	196 units		19	155	20
Duplexes	102 units	51	51	-	-
Apartments	62 units	26	38	-	-
Overall totals	360 units	77	106	156	21



Please refer to architect's House Quality Assesment (HQA) accompanying this SHD application for further information about project statistics, residential accomodation, public and communal open spaces provided, public facilities, private amenity, car parking, EV charging points, and bike parking.

1.1 GENERAL DESCRIPTION

The proposed development consists of 360no. homes, which will be served by high amenity land, neighbourhood pocket parks and a public park around the existing historic Moygaddy Castle tower. In addition, the application includes a Scout Den and a Childcare facility.

The 360no. homes envisaged achieves a varied mix of 196no. 2-storey houses, ranging from 2 to 4-bed dwellings, 102no. duplexes within 3-storey buildings including 1-bed ground floor units and 2-bed duplex dwellings, and 62no. apartments within two blocks located at both north-western and north-eastern corners of the scheme, comprising 1 and 2-bed homes.

This SHD application has explored higher density opportunities, as specified in national urban development policy, and particularly as required by the Meath County Development Plan (2021-2027) for the development's strategic location at the Maynooth Environs. We respectfully state that the achieved density, 45.6 units per hectare, is aligned with the national and local authorities' aspirations for density increase.

The layout has been developed in careful response to the specific requirements of the site. The applicable zoning and masterplan framework, topography and natural assets, embracing the mature woodlands and water courses of the Rye River and the Blackhall Little stream have been considered in the design response. Context opportunities have been adequately integrated to deliver an inclusive neighbourhood that will benefit from 6.99 hectares of new high amenity lands, which will form part of the open parkland with pedestrian and cycle paths creating strategic links to Maynooth town centre and to the wider Moygaddy Masterplan lands. These parkland areas will be complemented with the design of neighbourhood landscaped open spaces within the zoned lands, which are interconnected with each other and to the southern and eastern ecological corridors through an overall permeable public realm. In addition, the delivery of a Scout Den, a Childcare Facility, a Public Park and a Public Playground at the Moygaddy Castle environs will enhance the public amenity provided and contribute to bring a well served residential community.

The internal road network operates off a main street loop between the two access points envisaged, circulating through the central housing precinct and linking to traffic-calmed residential streets that give safe priority to pedestrian and cycling movement in a people-focused street design approach. The overall public realm will be finished in hard landscaping materials of high quality and durability, along with integrated soft landscaping and planting. Diverse streetscapes throughout the scheme will be overlooked by buildings primarily finished in brickwork in a consistent variety of colours and aesthetic finish.



Figure 1. Eastern and Midlands Growth Strategy and Strategic Connections. Source: EMRA.

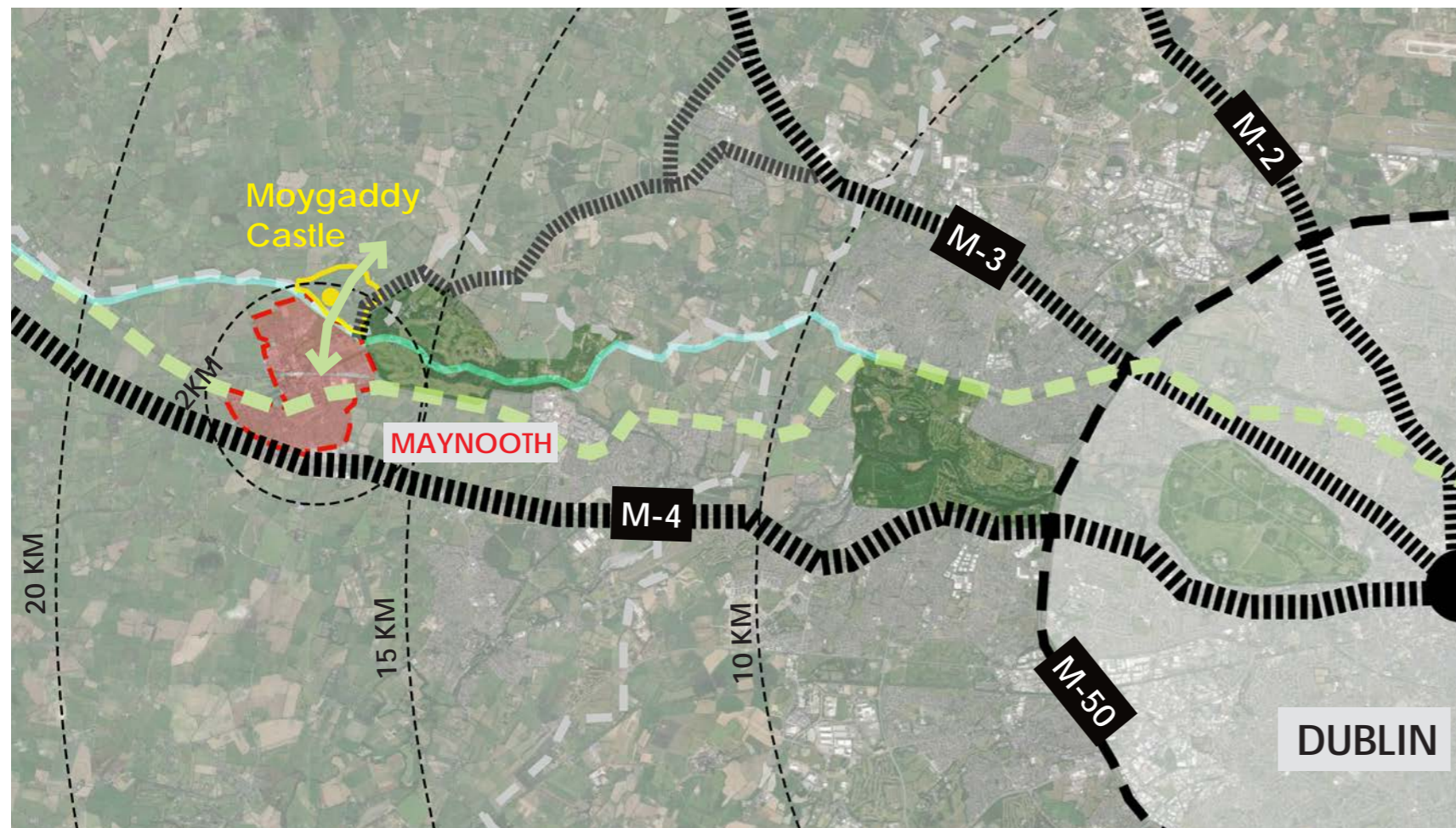


Figure 2. Location of SHD application within the Dublin Metropolitan Area. Source: MCORM, 2022.

1.2 SITE APPRAISAL

REGIONAL CONTEXT

The subject site is located in the Local Area known as *Maynooth Environs*, within Co. Meath which is to the north of Maynooth Town Centre, in the jurisdiction of Co. Kildare. As the proposed development has been designed to integrate into and become part of Maynooth Town. We have considered the location of this scheme within the Dublin Metropolitan Area Strategic Plan (MASP), within a radius of 20km distance to Dublin City Centre. Strategic residential and employment development corridors are identified in the MASP, while seeking to create sustainable compact communities with improved housing choice, access to social and economic opportunities, enhanced services and amenities for an envisaged resident population of some 1.65m people in the metropolitan area by 2031.

The subject site is located within the M-4 strategic corridor, 2km from the motorway and railway networks. This strategic location will help to attract new population and employment to the area. Strategic regional policy aims to support Maynooth Town in its sustainable development, in coordination with the delivery of strategic infrastructure including pedestrian and cycle linkages within the town and to the Royal Canal Greenway, DART expansion and new strategic road linkages forming part of the Maynooth Outer Orbital Route.

In addition, the designation of Maynooth as a key town by the EMRA Growth Strategy means the town will act as an economic driver for north Kildare and south Meath by providing for strategic employment at key locations to improve the economic base of the town and to provide for an increased number of local jobs (RPO 4.34).

It is recognised in the RSES that Key Towns should act as economic drivers and provide for strategic employment locations to improve their economic base by increasing the ratio of jobs to workers. While the number of people at work within the town is relatively high, a significant number of residents also commute to Dublin. The development of the Maynooth Environs is envisaged to be a natural extension to the town of Maynooth and it will act as a location for employment generation and the establishment of a self-sustaining 'live work' community. Therefore, it is envisaged that this residential development, which is a key component of the wider masterplan that includes employment lands, will shape a residential "live-work-play" community of local workers for Maynooth, within the mixed-use hub envisioned by the Moygaddy Masterplan, which is referenced in the next section (planning framework) of this design statement.



1.KILDARE BRIDGE



2.MOYGADDY STABLES



3.MOYGADDY CASTLE



- Masterplan (MP) boundary
- SHD Application boundary
- Neighbouring residential developments
- Relevant Community Amenities

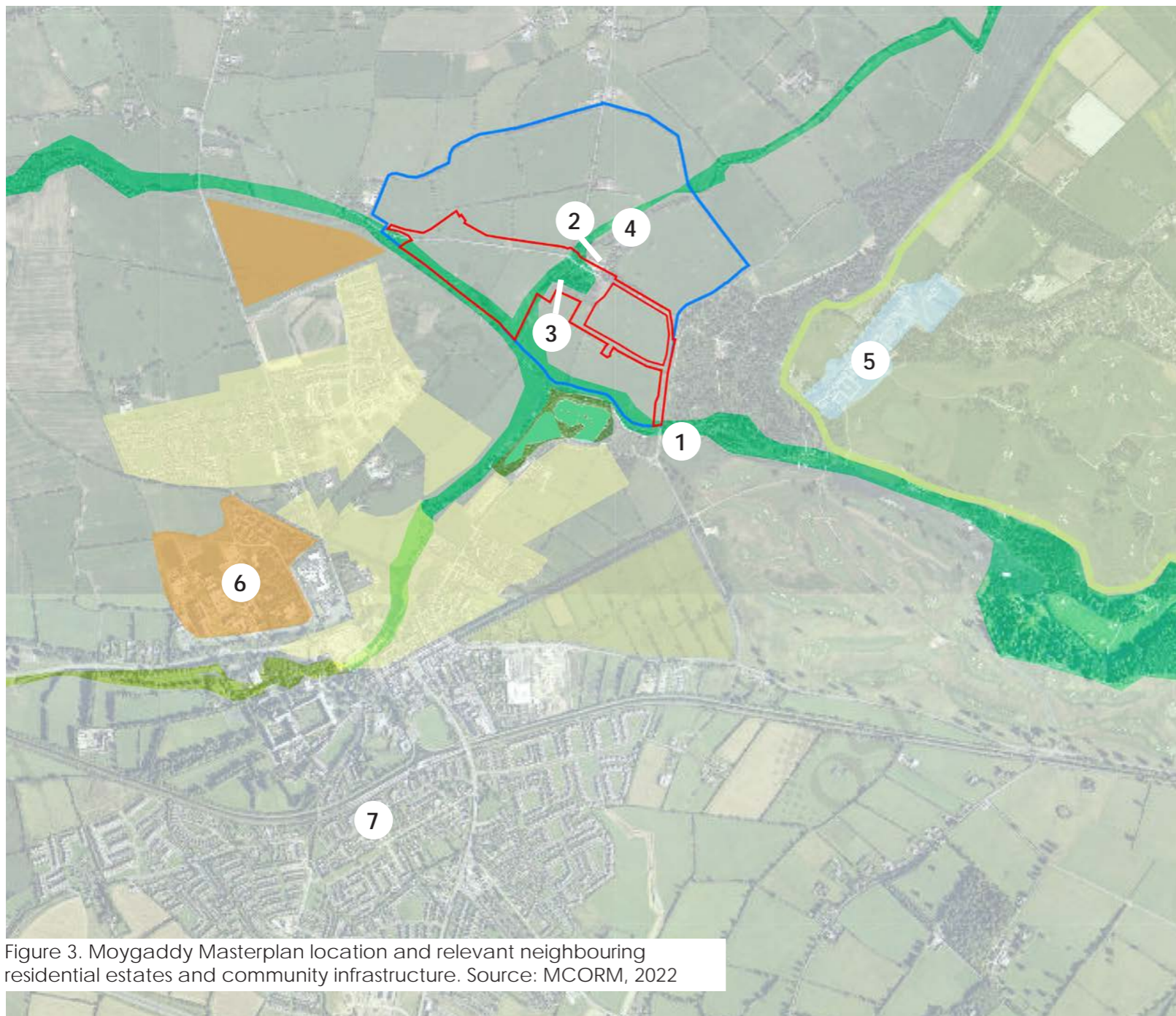


Figure 3. Moygaddy Masterplan location and relevant neighbouring residential estates and community infrastructure. Source: MCORM, 2022

LOCAL CONTEXT

The subject site is located in the south-western Moygaddy landholding, within the MP1 objective defined in the Meath CDP for the *Maynooth Environs* Local Area. It has a gross area of 17.52 ha., including 6.99 ha. of High Amenity Lands, which provides a unique opportunity to frame the 7.89ha of residential lands zoned along its south-eastern and south-western boundaries, enhancing existing green and blue corridors along the Rye Water and the Blackhall Little.

The subject site also contains heritage assets that have been fully considered and will be integrated within the phased development of the overall non-statutory Moygaddy Masterplan area. Within the scope of this application, the historic Moygaddy Castle tower will be a central feature of the public park delivered as one of the active spaces which form part of the High Amenity Lands and will serve this residential proposal and future phases of development within the Masterplan area.

Due consideration has also been given to the importance of Moygaddy House and Stables which link the Masterplan lands to the Carton House Demesne via L-6219 and L-2214-3 crossing the same from west to east. Neighbouring residential developments recently built to the south, such as Moyglare Hall and Mariavilla, are also appraised as the most proximate framework of local residential development.

Maynooth Town presents two main destination nodes, Maynooth University Campus and its Town Centre, which have been carefully considered in terms of the permeable and accessible connections provided, ensuring an effective road link with accessible pedestrian and bike routes from the site to the centre of town. please refer to the overall masterplan strategy provided in page 11, figure 6 for further detail on existing and proposed pedestrian and cycling links.

The “live, work, play” community envisioned for the overall Moygaddy Lands will enjoy the benefit of existing services and combine with Maynooth Town to form important synergies for a well-rooted and connected residential neighbourhood.



4.MOYGADDY HOUSE



5.CARTON HOUSE



6. MAYNOOTH UNIVERSITY



7. MAYNOOTH TOWN CENTRE

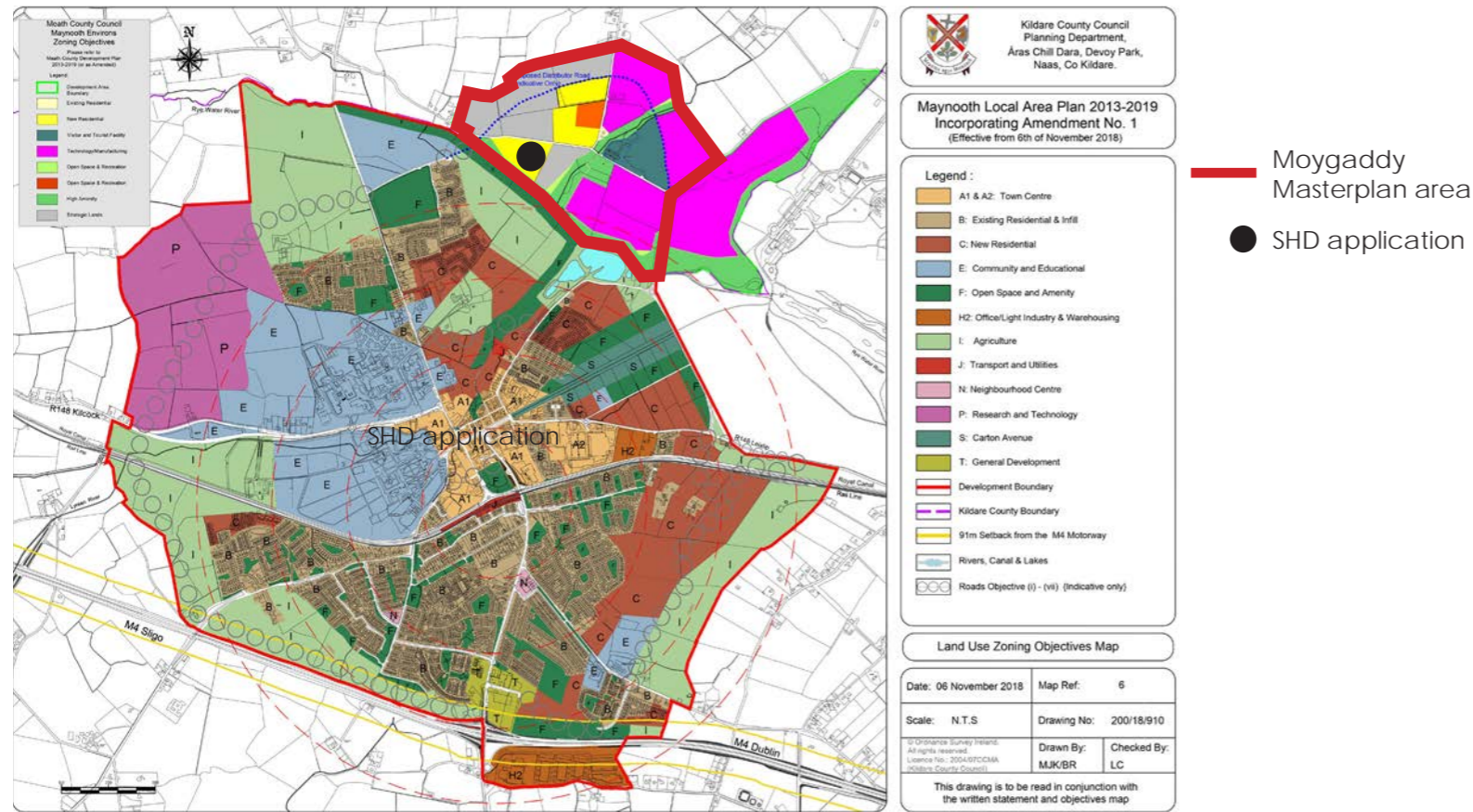


Figure 4. Maynooth LAP land-use zoning objectives map and location of Moygaddy Masterplan and SHD Application. Source: KCC and MCORM, 2022.

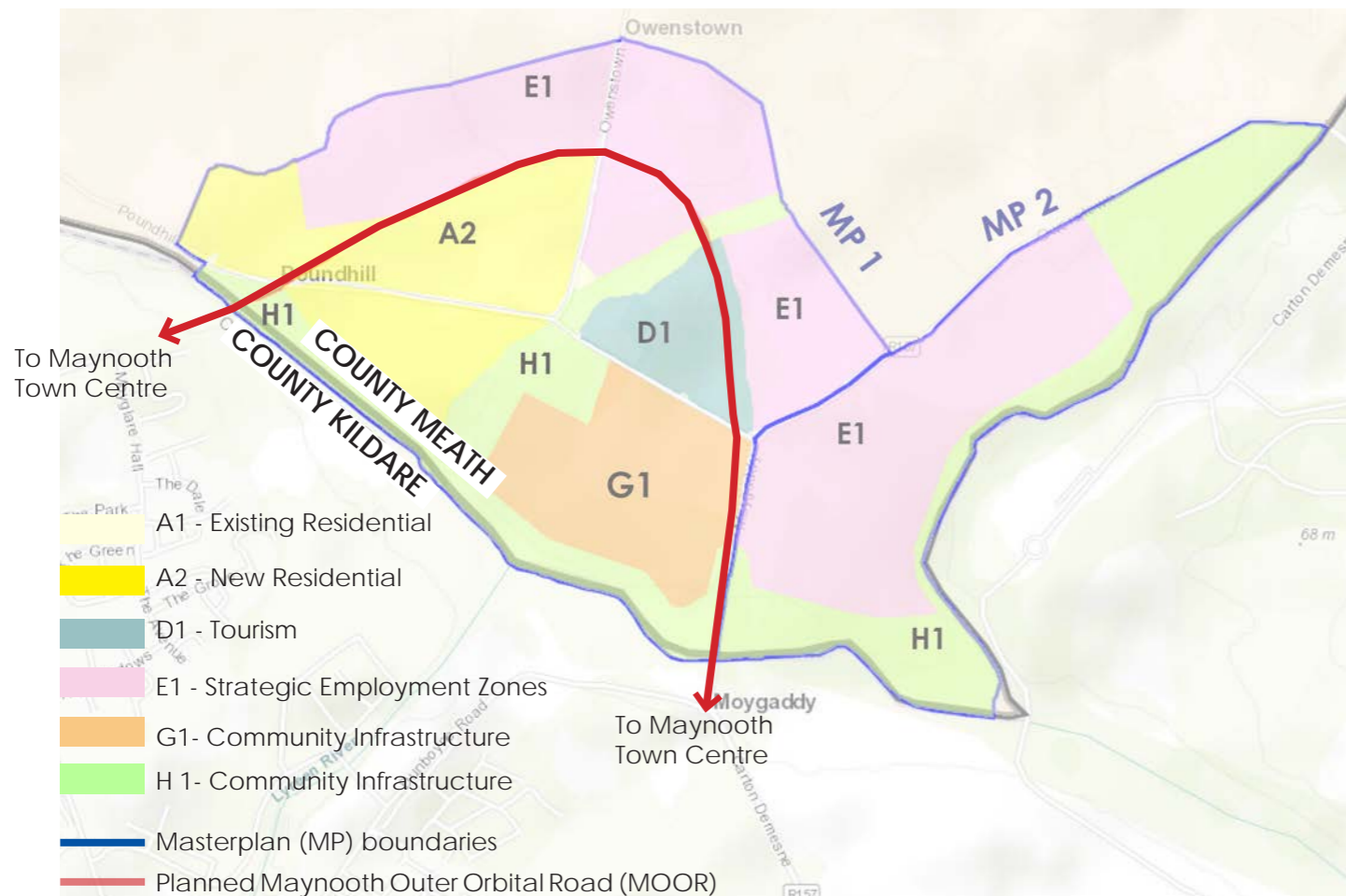


Figure 5. Meath County Council Development Plan for Maynooth Environs (2021-2027) and SHD boundary overlay. Source: MCC (through ArcGIS) + MCORM.

1.3 PLANNING FRAMEWORK

MEATH COUNTY DEVELOPMENT PLAN

The Meath County Development Plan (2021-2027) divides the Maynooth Environs Local Area into two master plan areas (MP1 and MP2). This application is located within MP1, including zoned residential and high amenity lands located at the southwest of the site, adjacent the River Rye and the boundary with County Kildare.

This SHD application site contains three land use zoning objectives, within the Meath CDP:

A2 Lands - New Residential

"To provide for new residential communities with ancillary community facilities, neighbourhood facilities and employment uses as considered appropriate for the status of the centre in the Settlement Hierarchy". Under this application 360 residential units are provided within 7.89 ha zoned residential lands.

H1 Lands - High Amenity

"To protect and improve areas of high amenity". 6.99 ha of high amenity lands are included within this application, including a public park in the vicinity of the existing historic Moygaddy Castle tower.

G1 Community Infrastructure.

"To provide for necessary community, social, and educational facilities". A portion of these lands adjoining the southern edge of the public park is included in this application to provide for a childcare facility (crèche) for the 360no. new homes in the scheme and to allow for expansion to support future residential developments within the lands at Moygaddy.

The site for the proposed creche has been chosen to ensure that this facility is centrally located between this SHD scheme and separate applications for new office and medical developments to the North and East which form part of the wider Moygaddy Masterplan.

KILDARE COUNTY DEVELOPMENT PLAN

The Moygaddy Castle lands are located in Co. Meath to the north of Maynooth Town Centre. Whilst the lands are not within the jurisdiction of Kildare, the current Kildare County Development Plan (2017-2023) recognises the importance of the lands and envisages the development of a multi-use hub, including residential, strategic employment, tourism and high amenity uses (see Figure 5 adjacent) at Moygaddy. In fact, material alterations to statutory maps within the Kildare CDP were included for consistency within the Meath CDP.

This proposal is therefore fully aligned with the current planning framework of Kildare County Council and, particularly, within the residential lands zoned in its CDP (as shown in figure 4, Land Use Zoning Objectives Map). Moreover, this development will be served by the Maynooth Outer Orbital Road (MOOR) that is proposed in the current Kildare CDP and, in this regard, the development of the Moygaddy Lands is strategic to ensure the continuity and completion of this infrastructure in the Co. Meath lands. The scheme will also provide a natural continuity to the neighbouring southern residential development (*Mariavilla*) and it will contribute to activating the northern edge of Maynooth with this residential scheme proposed and the uses outlined in the Meath County Development Plan.

Legend

- Railway Dublin - Sligo
- Relevant road network for Masterplan
- Main road network inputs
- Green corridors and routes proposed
- New roads objectives (Co.Kildare)
- New cycle/pedestrian routes (Co.Kildare)
- County boundary line (Co.Kildare)
- Maynooth Local Plan boundary line (Co.Kildare)
- Moygaddy Masterplan (MP) boundary
- A1 - Existing Residential
- A2 - New Residential
- D1 - Tourism
- E1 - Strategic Employment Zones
- G1 - Community Infrastructure

MOYGADDY MASTERPLAN (CO. MEATH) OVERALL STRATEGY

An integrated non-statutory masterplan has been developed by Sky Castle Ltd. in accordance with the MP1 objective of the Meath County Development Plan (2021-2027). This Masterplan has been developed in conjunction and with regard to the published policies and objectives envisaged for the Maynooth Environs in the Meath County Council Development Plan (2021-2027). The future growth and expansion of Maynooth town as a sustainable and attractive location to live, work and play is contingent upon the delivery of the following strategic infrastructure that will be facilitated by this non-statutory masterplan:

1. The installation of a **"new pumped rising main"** by Irish Water to address existing capacity constraints in the Maynooth plant.
2. The construction of the Northern section of the **Maynooth Outer Orbital Route (MOOR)** to address traffic congestion in Maynooth town.
3. The construction of a new **Elective Public Hospital** as part of the Government's new Slainte Care strategy, to enhance public healthcare in the region.
4. The construction of a **new Primary Care Centre** facility to provide non-acute healthcare services within the community, and for the benefit of Maynooth.
5. The creation of a **Strategic Employment zone** that can accommodate inward investment from Multinational Employers in the Med-Tech, Bio-Tech and Life Sciences sectors aimed at promoting employment in Maynooth, and the wider region which will be delivered in conjunction with strategic transport linkages and research collaboration with Maynooth University.
6. The delivery of a **mid-density (45.6units/ha) residential development** which will be delivered on a phased basis over the next 2 to 3 years, within a mixed-use community, complying with the 500 unit residential allocation established in the Core Strategy Housing Allocation of the Meath County Council Development Plan (2021-2027).

In accordance with the Meath CDP, the Moygaddy Masterplan is based on 4 main strategies that aim to develop an integrated, phased, connected, resilient and diverse live-work-play mixed use community. The overall strategy for this Masterplan at the Moygaddy lands is shown in figure6 opposite. The current residential application forms a key component of this masterplan framework and the relevance of the same provided in the next page.

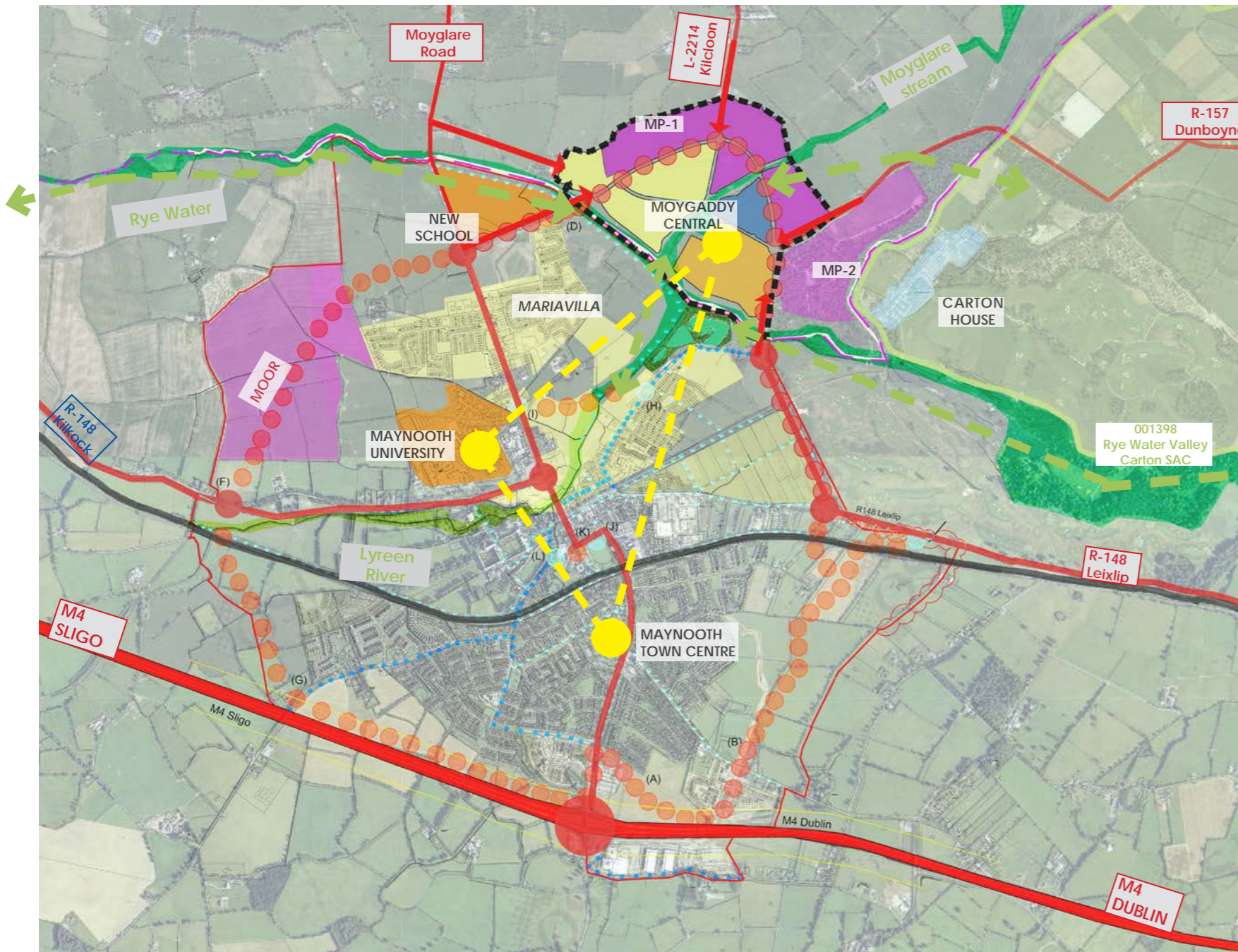


Figure 6. Overall strategy for the Moygaddy masterplan lands. Source: MCORM, 2022



1.4 ARCHITECT'S RESPONSE TO ABP OPINION

This section has been prepared in response to the Opinion received from An Bord Pleanála dated May 2022 (case reference ABP-312213-21) and the items raised during the tripartite meeting following the Stage 2 pre-application submission for this development at Moygaddy, Maynooth.

This scheme has been subject to a significant period of detailed design development from a multi-disciplinary Design Team over the past 18 months, working through a number of iterations before finalising the designs contained within this application. A number of amendments have been made to the overall scheme having due regard to the comments received and parameters set by planning objectives within the Meath County Development Plan.

The responses outlined below to the Opinion issued by An Bord Pleanála have been incorporated into the final design proposal. We have summarised the key items noted in the ABP Opinion below, which have helped to inform the design decisions:

1. Principle of Development

Further consideration/justification of the documents as they relate to the planned development strategy for the Maynooth Environs, the absence of a detailed masterplan (MP16) and/ or Local Area Plan and compliance with Objective MAY POL 1 and MAY OBJ 1 (Master Plan 16) of the Meath County Development Plan 2021-2027. The applicant shall ensure the further consideration/justification includes detailed information on the planned roll out and delivery of essential infrastructure, inter alia, the Maynooth Outer Orbital Road (MOOR) and wastewater treatment, necessary to accommodate any increased population from the proposed development and the overall capacity required for the masterplan area (MP16).

Response:

A detailed Masterplan drawing (Figure 7) and non-statutory Masterplan document has been prepared by MCORM Architects in conjunction with MKO Planning & Environmental Consultants following consultation with both Meath & Kildare County Council. The Masterplan carefully considers the specific objectives, policies and zoning guidelines set out within the Meath County Development Plan 2021-2027 and the Kildare County Development Plan (2017-2023), with the aim of delivering an attractive, sustainable and connected expansion of Maynooth town.

The Non Statutory Masterplan has been developed by the Applicant with direct reference to the Maynooth Environs LAP (2009) and the Meath & Kildare County Development Plans. This Masterplan is submitted to Meath County Council now in conjunction with this SHD application and it is accompanied by an EIAR and Flood Risk Assessment Report for the entire landholding in the Applicants ownership.

Please refer to the Architectural drawings accompanying this Stage 3 submission, in particular:

- PL101 SITE LAYOUT PLAN – MASTERPLAN
- 21006 MOYGADDY MASTERPLAN DOCUMENT









It is important to note the following considerations regarding the delivery of essential infrastructure for this SHD and the overall masterplan development:

- 1) This SHD seeks approval for the delivery of a section of the MOOR and the upgrade of the local road network to facilitate pedestrian and cycle connections. It should be noted that a separate planning application has been submitted to Meath County Council to seek approval for the entire MOOR.



Figure 7. Proposed Moygaddy Masterplan, to be read in conjunction with the response provided opposite and the Masterplan document submitted with this SHD application. Source: MCORM, 2022

Legend

-  MOOR
-  External Road network
-  Indicative pedestrian routes and connections
-  Pedestrian routes
-  Vehicular access points
-  Focal points (Public Park, Public Playground, creche, Scout den)
-  Public Open Spaces within zoned lands
-  New/enhanced bridges for motorised and non-motorised traffic

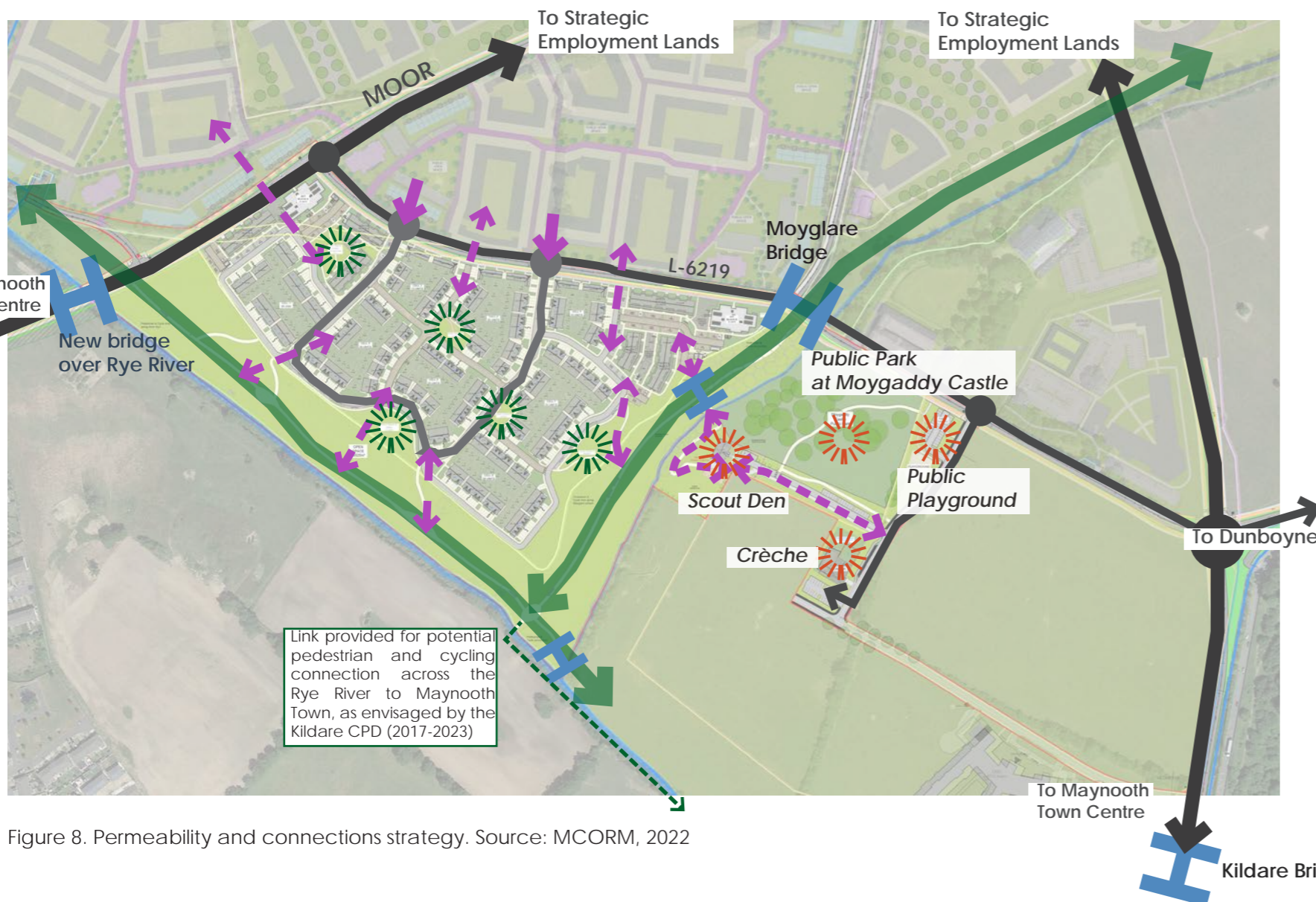


Figure 8. Permeability and connections strategy. Source: MCORM, 2022

2) The Applicant has worked closely with Irish Water to agree the route of the new strategic Maynooth Town High pressure sewer line, part of which transverses the eastern boundary of the Applicant's wider landholding. Irish Water will submit a separate planning application for this strategic infrastructure.

Irish water have issued a letter of confirmation of Design Acceptance for the Waste Water Infrastructure proposed for this SHD application which has been designed to provide capacity for the wider Masterplan area.

Please also refer to MKO Planning & Environmental Consultants and OCSC Consulting Engineers for the drawings and reports detailing the planned roll-out and delivery of the essential infrastructure to facilitate the proposed development. These documents contain detail design proposals and confirmation of design acceptance from relevant bodies (i.e Irish Water).

2. Permeability and Connectivity

Further consideration and/or justification of the documents as they relate to the movement of pedestrians and cyclists from the subject site into Maynooth Town and surrounding area. The submitted documentation should demonstrate clear pedestrian and cycle connectivity and the timescale, phasing and delivery of any infrastructure associated with the proposed pedestrian and cycle connectivity, including any third-party consents required for the works. For the cycle facilities that are proposed, the specific compliance with the particular requirements of the National Cycle Manual should be demonstrated by the documents.

Response:

The delivery of permeability and connectivity from the subject site into Maynooth Town and the surrounding area is a key part of the design brief which has been reinforced by the proposed pedestrian/cycle routes along the MOOR which connects into the existing R157 Dunboyne Road and Moyglare Hall Estate. As part of the development of the MOOR, upgrade works of the R157 and L6219 will also provide new, enhanced pedestrian and cycle routes to Maynooth Town. In addition, the site layout has been developed to integrate a strong streetscape with clearly defined housing cells that are linked to high quality landscaped areas, promoting pedestrian and cycle priority throughout. Shared pedestrian and cycle links have also been provided along the High Amenity zoned lands to the south and east of the subject site which ensure future connectivity to the adjacent lands whilst creating an enhanced river walk/cycle strategy.

We refer to Figure 6 and Section 2.2 Connections of this Architectural Design Statement for additional details of the design intent and to the Architectural drawings and documents accompanying this Stage 3 submission. The proposed pedestrian and cycle infrastructure will be delivered in tandem with the residential development. We respectfully suggest that this results in a residential scheme that is well connected and integrated with its surroundings whilst delivering a neighbourhood with a unique sense of place for the benefit of future residents of the scheme.

3. Design Strategy

Further consideration and/or justification of the documents as they relate to the design approach of the proposed development and the need for a high quality, strong urban edge which integrates effectively along the proposed Maynooth Outer Orbital Road (MOOR). The further consideration/justification should address the open space strategy, inter alia the need passive surveillance and functionality of open space (in particular the proposed communal courtyard spaces) and the future management and maintenance of all open space areas. Particular regard should be had 12 criteria set out in the Urban Design Manual which accompanies the Guidelines for Planning Authorities on Sustainable Residential Development in Urban Areas (May 2009) and the requirement for good design and the inclusion of a sense of place.

Response is provided overleaf

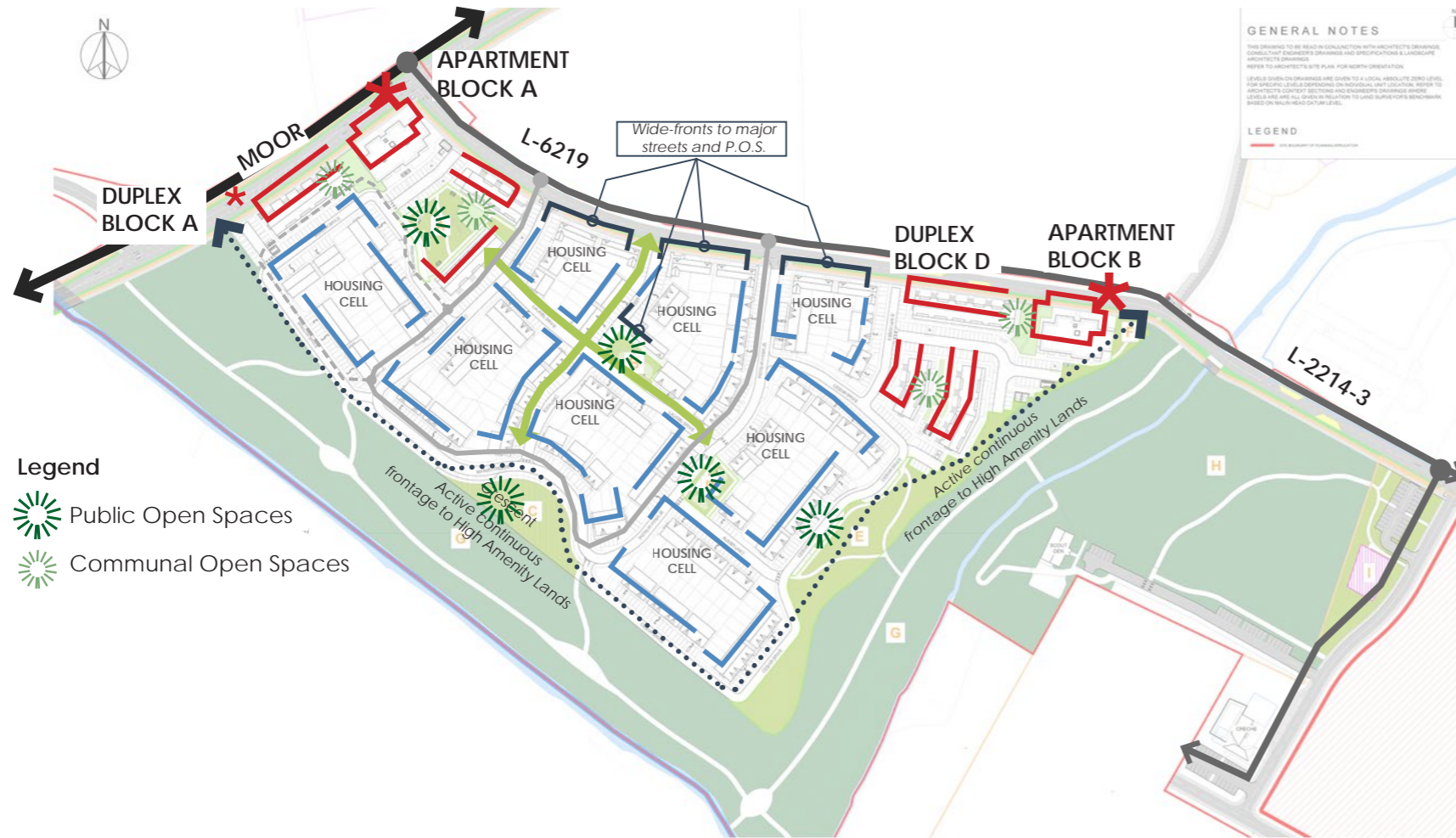


Figure 9. Site layout diagram showing a legible urban structure of housing cells framed by prominent cornets at north-west and north-east corners and bounded by lands of high amenity along the south and east of the site. Source: MCORM, 2022.



Figure 10. CGI illustrating strong built edge to the MOOR and junction with enhanced road L-6219, composed by a distinct 4-storey apartment block and adjacent duplex buildings to both sides. Source: 3DDB, 2022.

Response:

The design has addressed the unique characteristics of the site creating a compact, strong urban edge of higher-density typologies consisting of apartment and duplex buildings to the east and west 'corners' of the subject site along the proposed MOOR and L6219. As you move to the central portion of the site the scale of the scheme reduces to a more traditional mix of housing typologies, bound to the south and east by the High Amenity lands, as shown in the urban design strategy below (Figure 10), which results in the variety illustrated in elevations provided opposite.

The development also benefits from a number of distinct character areas which help to provide a diverse mix of typologies, catering for a wide range of households whilst enhancing the overall visual interest of the scheme. This mix of typologies also helps to create active frontage within the public realm through the use of gable entry units and variations to elevational treatments, which in turn ensures passive surveillance of all open space areas is achieved.

Within the typologies' mix, a specific design approach has been implemented on the Duplex and Apartment buildings by providing two active 'fronts'. Access to the one-bedroom dwellings in the Duplex building is provided on one side, whilst the two-bed dwelling is accessed from the other. Further consideration has been given to provide gable-entry Duplex units in prominent locations, creating a third 'front'.

Similarly with the Apartment buildings, the 'front' elevation is situated along the MOOR and L6219 whilst the car-parking facilities are accessed from the internal side of the scheme with additional access to the apartment building creating a second 'front' for the benefit of the residents. Please refer to figure 9 for the overall site layout strategy, in addition to 10 and 11 demonstrating the configuration of a strong built frontage to the MOOR.

Section below



Please note section below showing strong frontage of duplex building overlooking the planned road. Please note the reinforcement of this strategy with the apartment building located adjacent (elevation above and CGI opposite), addressing the road junction between the MOOR and the L-6219.

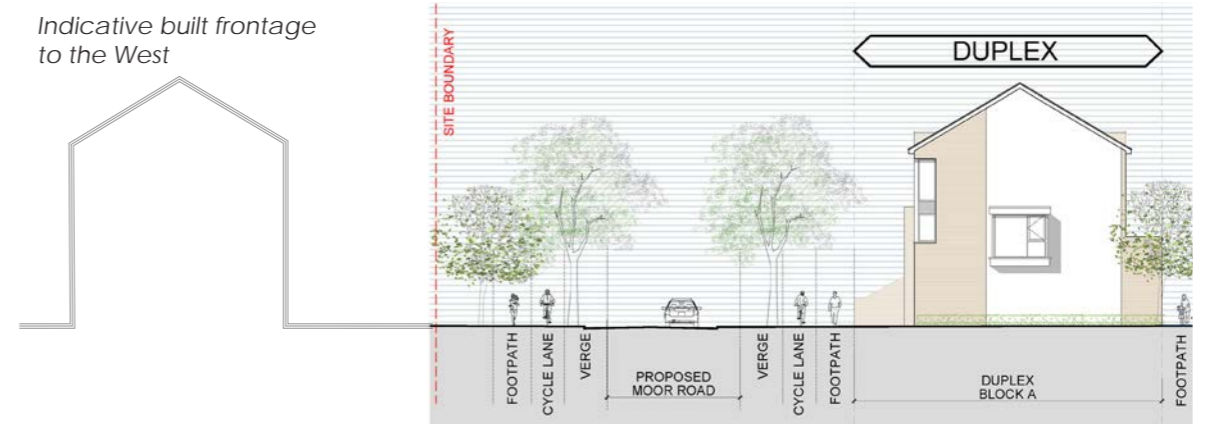
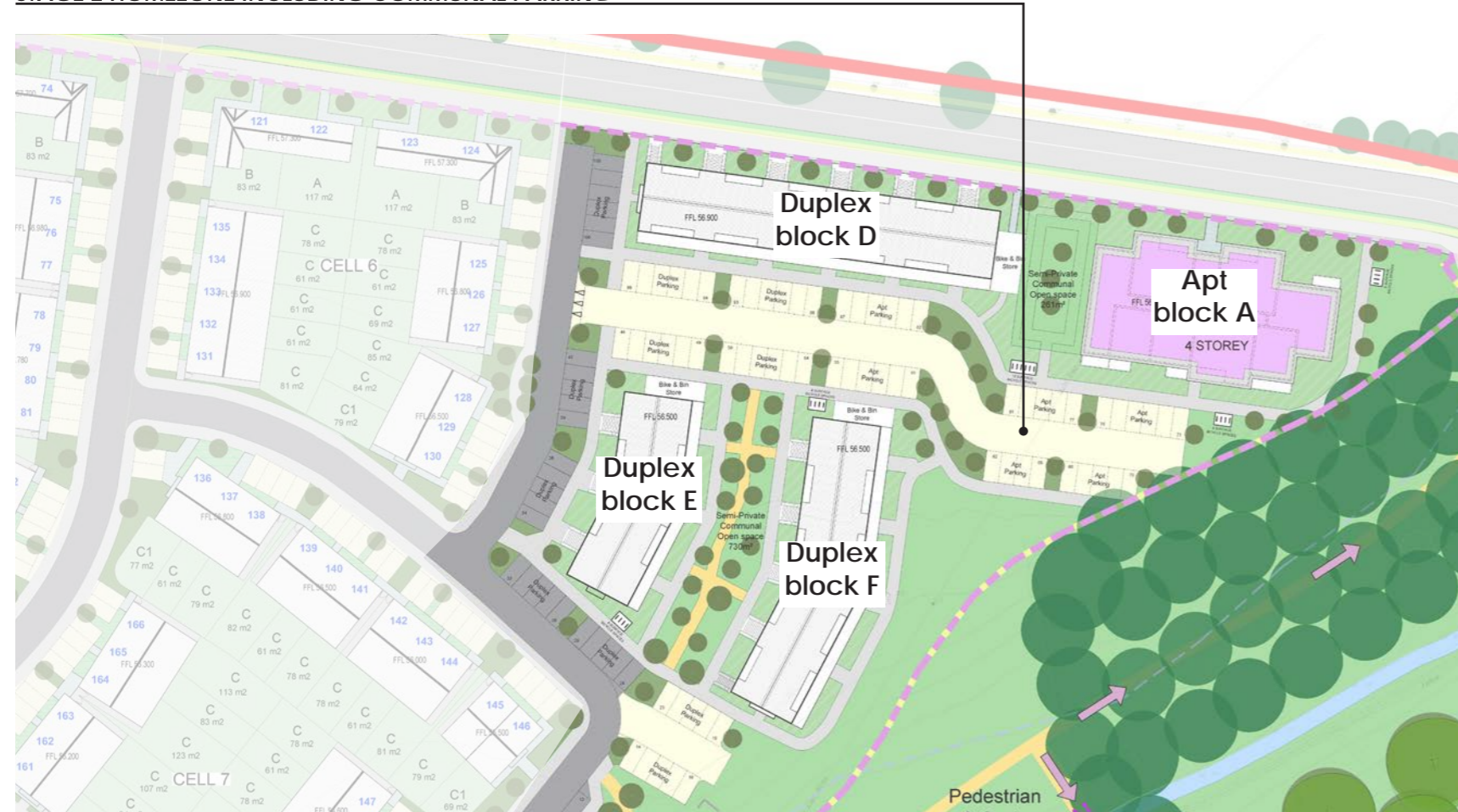


Figure 11. Cross section through the MOOR and elevation fronting the MOOR including duplex and building apartments configuring a strong frontage to the road at the access to the Moygaddy lands. Source: MCORM, 2022.

STAGE 2 HOMEZONE INCLUDING COMMUNAL PARKING



In addition to the High Amenity lands which account for 36% of the gross site area, the scheme benefits from a number of high quality landscaped Public Open Spaces in the form of "pocket parks", which act as focal points and wayfinders within the housing fabric housing cells, linking to the larger parkland areas.

These public spaces and communal open spaces serving the Apartment and Duplex units have been designed with a user-friendly approach, conveniently located adjacent to each building whilst also providing clear visual breaks in the built edges of the development which also helps to improve the overall streetscape. Each of the shared communal areas have been further developed from Stage 2, with careful consideration given to the location and design of the shared Bin & Bicycle storage areas within the scheme. Further information in this regard is given overleaf.

Following feedback received during the Tripartite meeting on 5th May 2022, an additional turning head was introduced to the north-east section of the development to facilitate turning movements for refuse and emergency vehicles and reduce the extent of cul-de-sac previously indicated at Stage 2. This also created an opportunity to redistribute the parking spaces that were previously adjacent to Apartment Block B, providing allocated parallel spaces in closer proximity to Duplex Block F as a result.

By redistributing the spaces along an extended turning head, the immediate landscape surrounding the Apartments & Duplex becomes much softer with greater diversification of the types of spaces provided and access to communal facilities such as the secure Bicycle and Bin stores located in the area.

STAGE 3 ENHANCED HOMEZONE WITH ENHANCED COMMUNAL PARKING AND ACCESSIBILITY TO RESIDENTIAL BUILDINGS

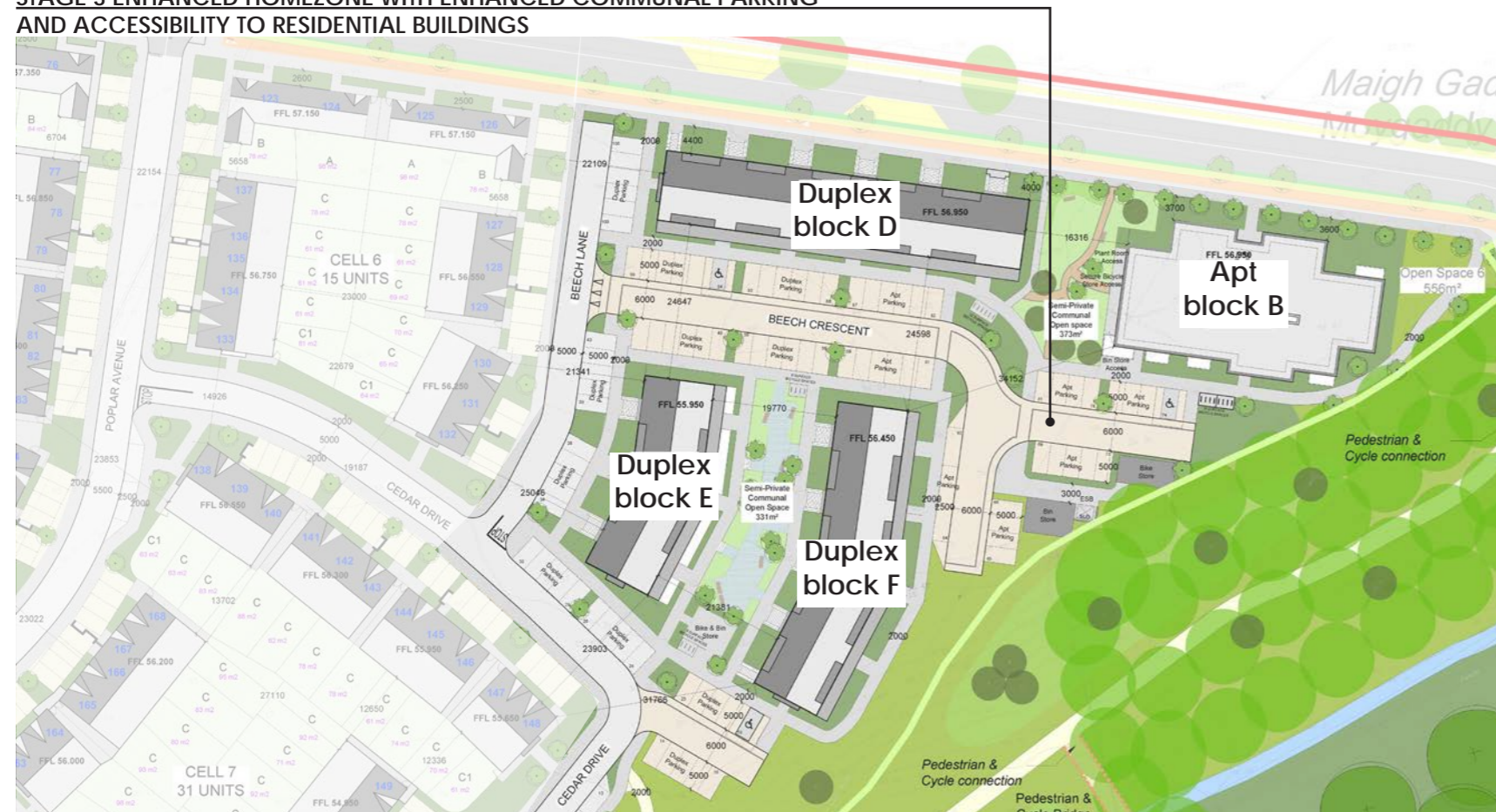


Figure 12. Detail of site layout Stage 2 and Stage 3) enhancement of communal areas and services provided (bins and bike stores) at the north-eastern corner, along with improvement of street layout. Source: MCORM, 2022.

Figure 13. Key plan indicating location of detail opposite. Source: MCORM, 2022

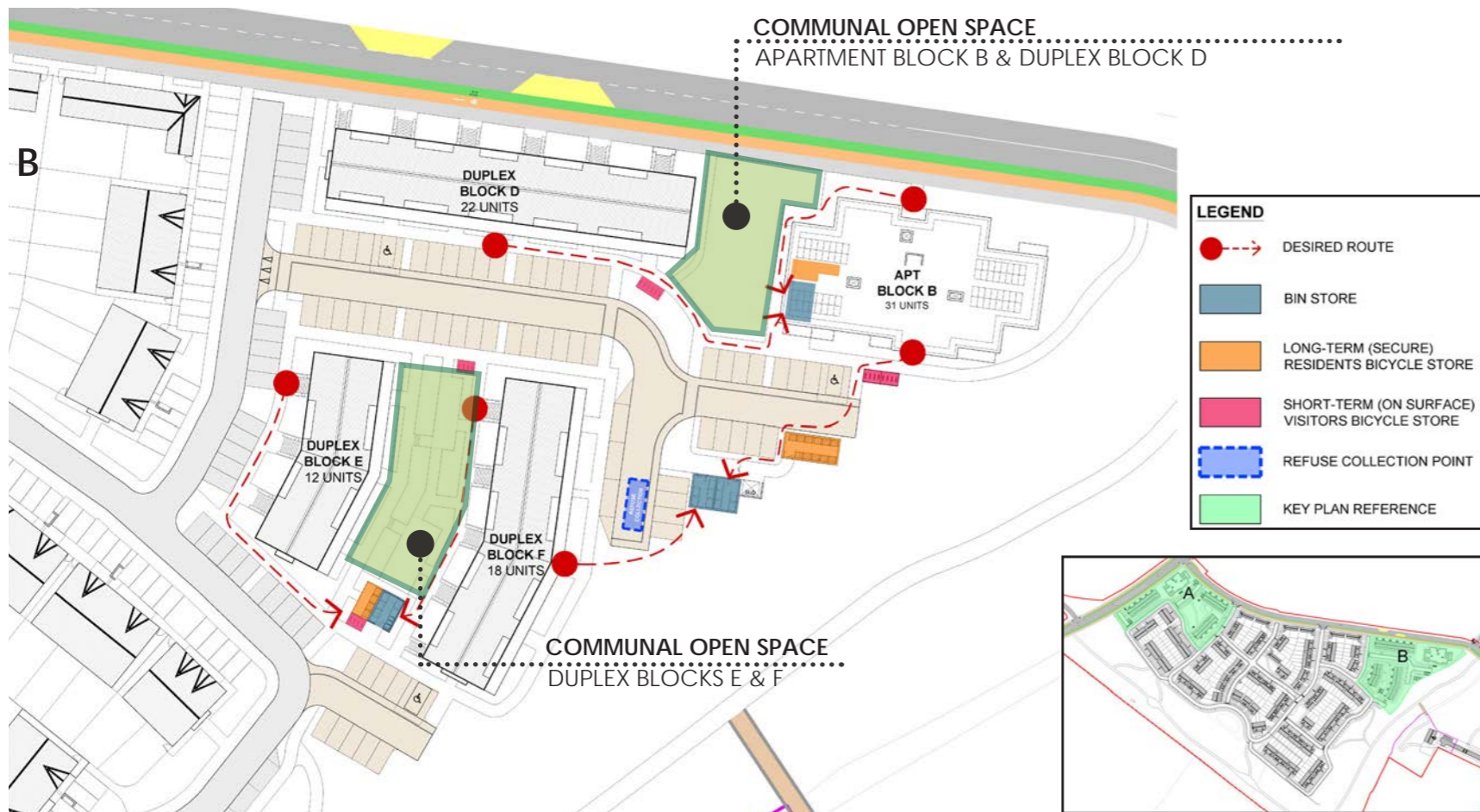
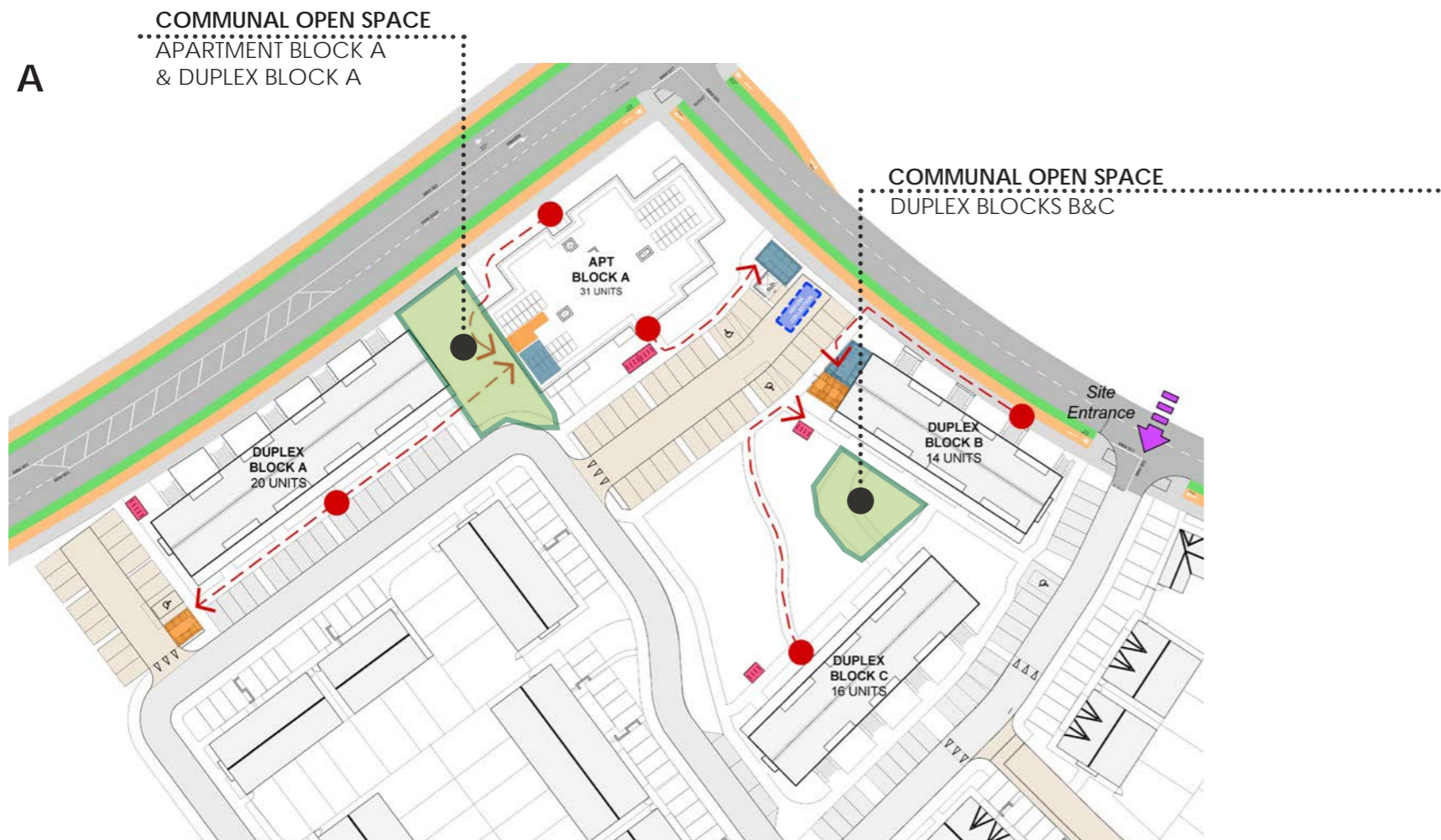


Figure 14. Site layout details, indicated in green in key plan, showing enhanced functional and visual integration of communal open space and services within the overall built environment proposed at north-western and north-eastern areas of apartments and duplex buildings. Source: MCORM, 2022.

In order to provide more attractive and accessible communal areas specifically – with particular reference to the areas between Duplex Block A & Apartment Block A (west of the site) and Duplex Block D & Apartment Block B (east of the site), a ground floor apartment unit has been removed from each apartment block to facilitate the housing of a secure Bins and Bicycles store. This enhances the quality of these communal spaces by removing a single-storey external store which was previously incorporated at Stage 2.

Additional consideration has also been given to the shared communal areas between Duplex Block B & C and also Duplex Block E & F which have been further detailed for Stage 3. In all cases, the Stage 2 design referenced external bin and bicycle storage structures along the gable of each building. However, careful consideration has now been given to the specific placement, scale and form of each service area to ensure the communal areas are enhanced. See figure 5.

In response to the future management and maintenance of all open space areas, we have prepared a drawing identifying the Taking in Charge strategy proposed to the Local Authority which corresponds to the scheme's public realm as indicated on architect's drawings PL109 & PL110.

All public and communal open spaces will be finished on high quality hard and soft landscaping materials. Please refer to the Building Lifecycle Report and Landscape Architect Report accompanying this application for further details.

Please refer to Section 2.7 and 2.8 within this Architectural Design statement for additional details of the Open Space strategies and considerations which have been developed to highlight the design approach to create a strong built edge and integration of the overall scheme. Section 2 has been specifically prepared to address the 12 criteria set out in the Urban Design Manual.



Figure 15. CGI illustrating integration of public and communal open areas within the overall landscaping layout, overlooked by adjacent duplex blocks. Source: 3DDB, 2022.

2 URBAN DESIGN RATIONALE



Figure 16. Site photographs. Source: RMDA Landscape Architects (2021).

2.1 CONTEXT

The existing Green and Blue infrastructure defines the site context and has been essential for developing the appropriate site layout and settlement strategy, and determining how the proposed residential neighbourhood would be laid out. The surrounding environment acts as one of the drivers for the urban design concept, as it provides opportunities in terms of sustainable green infrastructure and strategic linkages with Maynooth town and to the other mixed uses planned within wider the Moygaddy landholding.

In this regard, the existing linear blue and green systems that bound the southern edges of the development along the Rye River and Blackhall Little stream, which are valuable assets both in terms of their natural value and their zoning as High Amenity Lands. These green areas equate to 36% of the site's gross area (4.4 hectares). This is a unique opportunity to create visually attractive and usable parkland boundaries. To take the maximum benefit from this rich environment, the multidisciplinary design team has developed the following nature-based solutions:

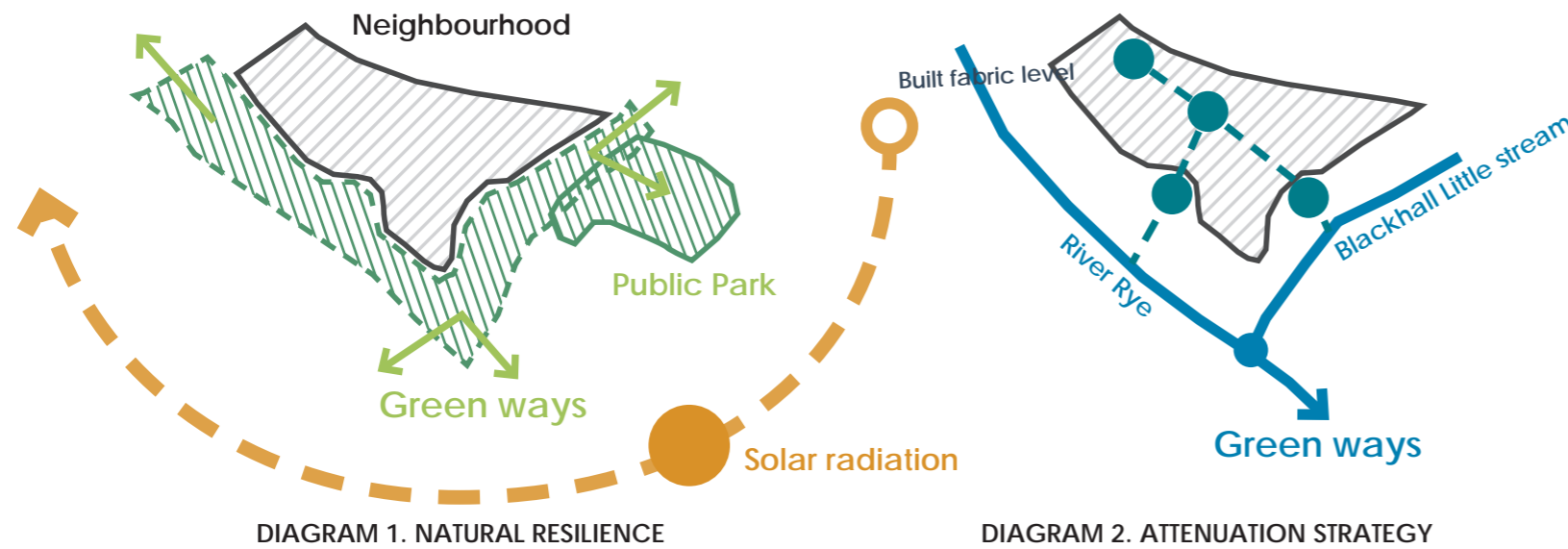


Figure 17. Moigaddy Masterplan overall strategy. Source: MCORM

1. A flood-resilient linear park with a nature-based green infrastructure is envisioned, which acts as the "green belt" surrounding the neighbourhood and includes active and passive green spaces, as shown in natural resilience diagram no.1. Within this natural environment, a local riverside park is proposed which links the environs of the existing Moygaddy Castle tower, integrating this heritage feature into the landscaped amenity of the development. This park forms part of this SHD application but it will also be an attractive focal point for the overall masterplan lands. Detailed design of this park land is addressed by Landscape Architects Ronan MacDiarmada & Associates.

2. A surface water attenuation strategy is proposed based on an interconnected system of underground attenuation tanks and a dry swale, all of which are integrated into open space areas, and linked to existing water courses (Blackhall Little Stream and Rye River), as indicated in diagram 2. This strategy is explained in further detail in the supporting submission by OCSC Consulting Engineers.

3. All buildings proposed are located in Flood Zone C outside the flood plain, as indicated in site photograph no.2, which shows the built fabric level as seen from the lower level zoned high amenity lands.

The network of linear riverside park links will deliver connections from the subject site to Maynooth Town centre and it will link the new residential community with future phases of development within the Moygaddy masterplan lands. The residential scheme has been designed to take maximum advantage of this green and blue infrastructure, in order to maximise permeability across the lands for walking and cycling journeys. Connections will link the riverside walks into the scheme by organic footpaths. They will also enable connectivity of the scheme to neighbouring lands through a network of green routes and cycle and pedestrian friendly permeable ways, well defined and surrounded by existing mature trees and hedgerows. Please see next section overleaf for further reference on site linkages (Figure 18).

The external connections of the scheme will be clear and legible. The scheme embraces the opportunity of creating ecological corridors along the River Rye and



Figure 18. Site strategy diagrams. Source: MCORM, 2021



Location of Moygaddy Castle Phase 1A



Figure 19. Rye River course Source: RMDA Landscape Architects.

Figure 20. E-W link (road L-6219) at bridge over the Blackhall Little stream. Source: Google Maps.

2.2 CONNECTIONS

Blackhall Little, which facilitates the delivery of pedestrian and cycling connections to future developments at the Moygaddy lands towards the East and to Maynooth Town Centre towards the South.

The regional and local planning policy favours the construction of the MOOR, which will be the main link to Maynooth town centre for car and public transport journeys.

This scheme is framed both by the northern section of the MOOR and the improved east-west link, roads L-6219 and L-2214-3 (see Figures 20 & 21), crossing along the Moygaddy lands connecting both sides of the MOOR as it turns back into the Maynooth Town centre and the road network already existing at the Kildare bridge. It will also provide for access to the proposed crèche, located adjacent to the public park at the Moygaddy Castle tower lands.

The development provides for the upgrade of the road network to ensure it is rendered fully accessible for pedestrians, cyclists and vehicles, and so that there are direct linkages created between the new residential community and the other planned Tourism, Medical and Office developments, which form part of the Masterplan. The location of the Public Park and Crèche have been chosen to ensure that they can be easily accessed by both the residential community and so that they will also serve future employment land phases in due course.

This application also envisages the construction of a new vehicular bridge over the Rye River as part of the MOOR, indicated in diagram opposite, and a new pedestrian and cycle bridge on the local road over the Blackhall Little Stream to enhance connectivity between the new residential community, the crèche, the proposed employment and medical developments, and the new public park and playground at Moygaddy Castle.

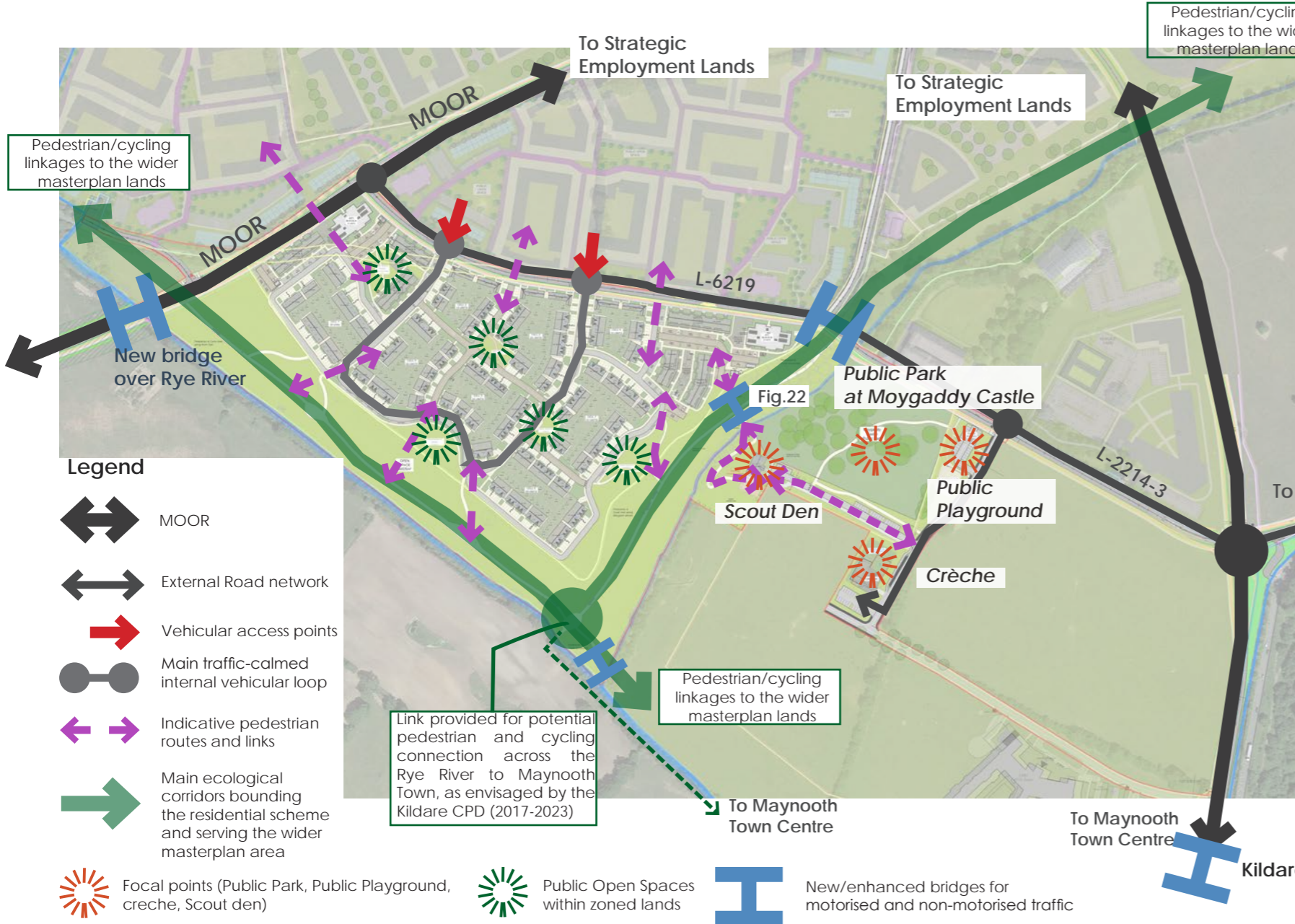


Figure 21. Diagram showing strategy for external connections and internal permeability linking focal points and external routes. Source: MCORM, 2022

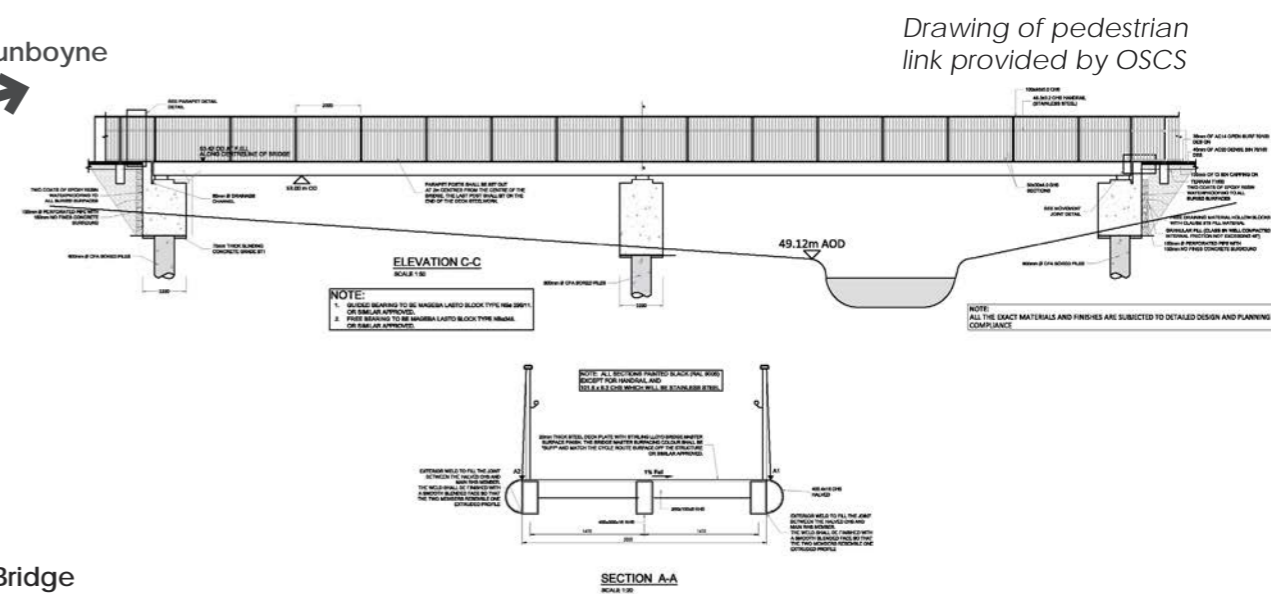


Figure 22. Pedestrian connection over the Blackhall Little stream, connecting the residential neighbourhood with the public facilities located East. Source: OSCS, 2022.



Figure 23. Residential mix diagram. Source: MCORM, 2022.

2.3 INCLUSIVITY

A rich variety of dwelling types is envisaged in a mix of 2-storey houses, 3-storey duplexes and 4-storey apartment buildings, including 1, 2, 3 and 4 bed homes. The scheme will therefore cater for a wide variety of social groups and households, creating an inclusive residential community that will be also served by a Childcare Facility, a Scout Den, and a Public Park and Playground. All of the aforementioned will be integrated within an accessible walkable public realm network to provide easy access for all future residents. Public open spaces and parkland areas, ranging in scale and character, will provide for diverse intergenerational activities, thus contributing to the inclusivity of the scheme, as detailed in the below.

As illustrated opposite, the residential mix provided achieves a balanced mix of 1 to 4-bed dwellings. House types proposed include 2 to 4-bed layouts while duplex and apartment buildings comprise 1 and 2-bed typologies. In particular, duplexes include own-door 1-bed units at ground floor level and 2-bed duplex types above.

The overall mix includes social/affordable housing having regards to the applicable planning policy on this item, designating 10% of the residential homes as Part V dwellings. These units have been chosen to range in size and typology, in addition to its uniform distribution across the scheme, including 2-bed and three bed houses at different locations and duplex and apartments in the north-western and north-eastern precincts of the scheme.

The residential accommodation provided will be fully Part M compliant with accessible entrances to all units. Technical Guidance Documents have been followed particularly where accessibility is concerned. Physical and visual barriers have been avoided and full passive surveillance has been designed into the layout in order to ensure all public realm areas are passively overlooked. Street layouts, footpath arrangements, street and verge planting combine to allow pedestrians and cyclists to easily navigate the scheme through the different character areas proposed.

Legend

<p>HOUSES (13 UNITS)</p> <ul style="list-style-type: none"> - 3-BED HOUSE TYPE C (10 UNITS) - 2-BED HOUSE TYPE E (3 UNITS) <p>DUPLEX BLOCK A-F (12 UNITS)</p> <p>DUPLEX BLOCK A (2 UNITS)</p> <ul style="list-style-type: none"> - 1-BED DUPLEX GROUND FLOOR APARTMENT (1 UNIT) - 2-BED DUPLEX APARTMENT (1 UNIT) <p>DUPLEX BLOCK B (2 UNITS)</p> <ul style="list-style-type: none"> - 1-BED DUPLEX GROUND FLOOR APARTMENT (1 UNIT) - 2-BED DUPLEX APARTMENT (1 UNIT) <p>DUPLEX BLOCK C (2 UNITS)</p> <ul style="list-style-type: none"> - 1-BED DUPLEX GROUND FLOOR APARTMENT (1 UNIT) - 2-BED DUPLEX APARTMENT (1 UNIT) <p>DUPLEX BLOCK D (2 UNITS)</p> <ul style="list-style-type: none"> - 1-BED DUPLEX GROUND FLOOR APARTMENT (1 UNIT) - 2-BED DUPLEX APARTMENT (1 UNIT) <p>DUPLEX BLOCK E (2 UNITS)</p> <ul style="list-style-type: none"> - 1-BED DUPLEX GROUND FLOOR APARTMENT (1 UNIT) - 2-BED DUPLEX APARTMENT (1 UNIT) <p>DUPLEX BLOCK F (2 UNITS)</p> <ul style="list-style-type: none"> - 1-BED DUPLEX GROUND FLOOR APARTMENT (1 UNIT) - 2-BED DUPLEX APARTMENT (1 UNIT) 	<p>APARTMENT BLOCKS A & B (11 UNITS)</p> <p>APARTMENT BLOCK A (7 UNITS)</p> <ul style="list-style-type: none"> 1-BED APARTMENT TYPE C1 (4 UNITS) 2-BED APARTMENT TYPE A1 (3 UNITS - FIRST TO THIRD FLOOR) <p>APARTMENT BLOCK B (4 UNITS)</p> <ul style="list-style-type: none"> 1-BED APARTMENT TYPE C1 (4 UNITS)
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PART V TOTALS			
1-BED	2-BED	3-BED	TOTAL
14	12	10	36
39%	33%	28%	100%

- Public Open Spaces within zoned lands
- Communal Open Space for apartments and duplexes



Figure 24. Part V diagram, including location of public and communal open spaces. Source: MCORM, 2022.



Indicative image of riverside park (High Amenity Lands)



CGI of Pocket Park 1 (A) and duplex communal open space

The scheme incorporates a large range of accessible and attractive open spaces which are designed according to the needs of the diverse community envisaged. In addition, the open space network embraces the sylvan character of the site, making existing woodlands and water courses accessible for all, particularly at the southern and eastern high amenity lands (indicated with a "G" in the map and photographs opposite).

Communal open spaces for duplexes and apartments, as highlighted in light green opposite, are located adjacent to their correspondent residential buildings, with accessible routes from their legible entrances. These semi-private landscaped areas are designed to cater for all generations of residents and care has been taken to ensure their integration with the overall open space network and, at the same time to ensure they are distinguished from the public realm enclosed by the surrounding built fabric and soft landscaped features, such as low or mid-rise planting.

We respectfully state that the landscaped open spaces, including public and communal areas, and residential typologies proposed are a strong contribution to an overall inclusive development with a diverse user focused design approach.



Indicative play area within the landscaped open space network



Indicative photomontage of Moygaddy Castle Public Park

Figure 25. Inclusive public and communal open space network and images. Source: MCORM, 2022.

2.3.1 CHILDCARE FACILITY

The scheme includes a crèche that can accommodate 61 no. children and which will serve this first phase of Moygaddy Castle. It can be potentially extended to serve future residential developments within the masterplan area, as indicated in figure 27. The childcare facility proposed is located accessible to both the current scheme and future office and medical developments on adjoining lands through direct road developments cycling and pedestrian links. Vehicle access is provided by the upgrading of roads L-6219 and L-2214-3, including the enhancement of the bridge across the Blackhall Little stream, and link street to the south between the envisioned public park and community infrastructure lands. In addition, to facilitate walking and cycling links a number of pedestrian paths and cycle lanes have been proposed through the scheme and through the riverside parkland crossing the Blackhall Little steam, arriving to the public park at the Moygaddy Castle historic tower and adjacent childcare facility proposed at that location, within the lands zoned as community infrastructure.

The crèche is designed to allow for future extension, as indicated with dashed line in the plan below. All façades will be finished in a combination of trowel finished sand/ cement render with coloured infill panels to give a particular character at points of the building, such as the entrance or at openings (see Figure 27). The required area of the childcare facility is calculated according to the overall residential mix, as shown in the table below:

Crèche (20 child spaces per 5sqm per 75 dwellings) 61no. children	
1-bed	Excluded
2-bed	55 units (50%)
3-bed	155 units
4-bed	20 units
Area required	305 sqm
Overall area	514 sqm

Figure 26. Crèche area calculation. Source: MCORM, 2022..

It is envisaged that the creche, public park and playground will act as a central community hub offer within the wider masterplan.



Figure 28. Key plan indicating central location of creche. Source: MCORM, 2022.

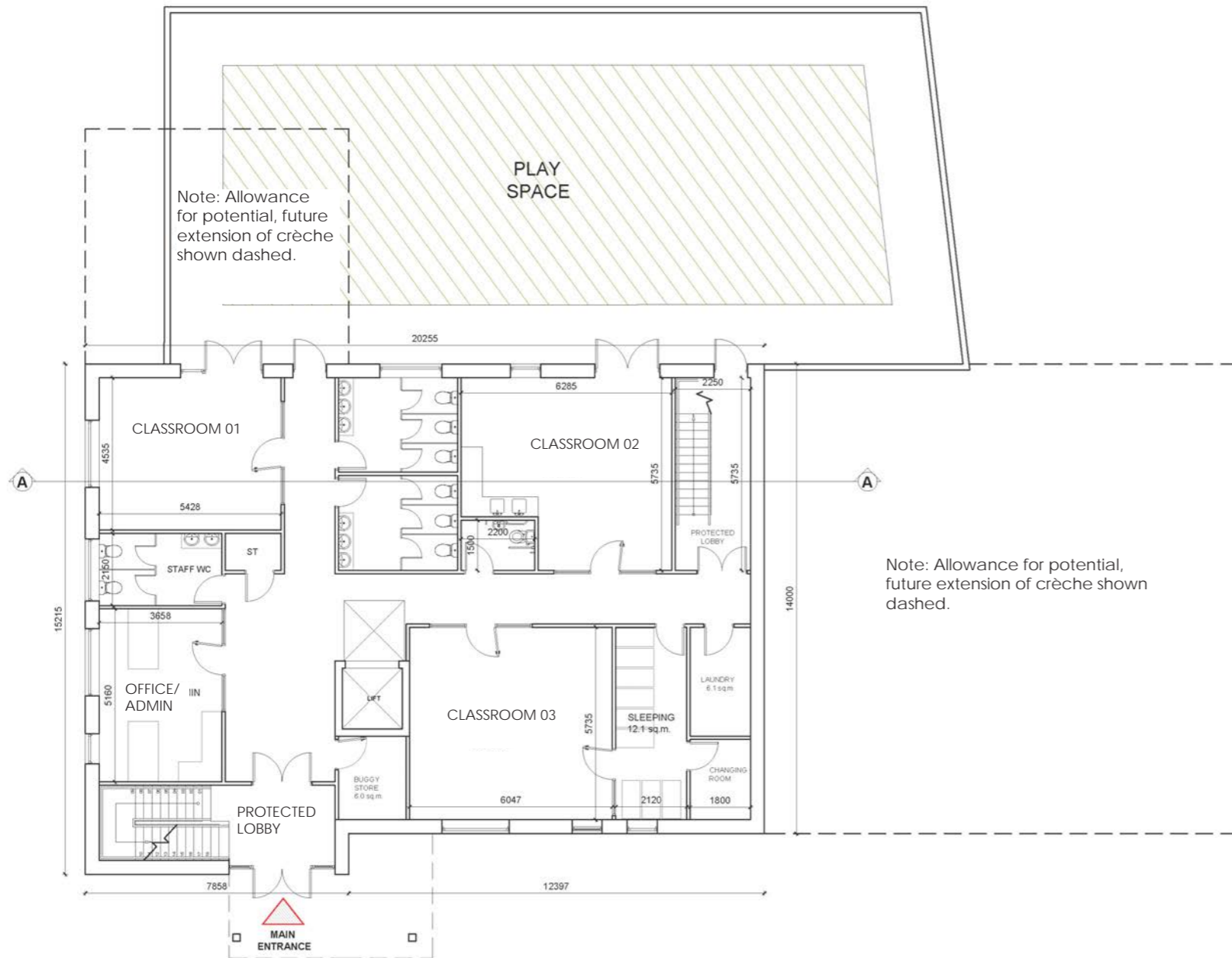


Figure 27. South-East (front) elevation and ground floor plan of creche proposed. Source: MCORM, 2022..

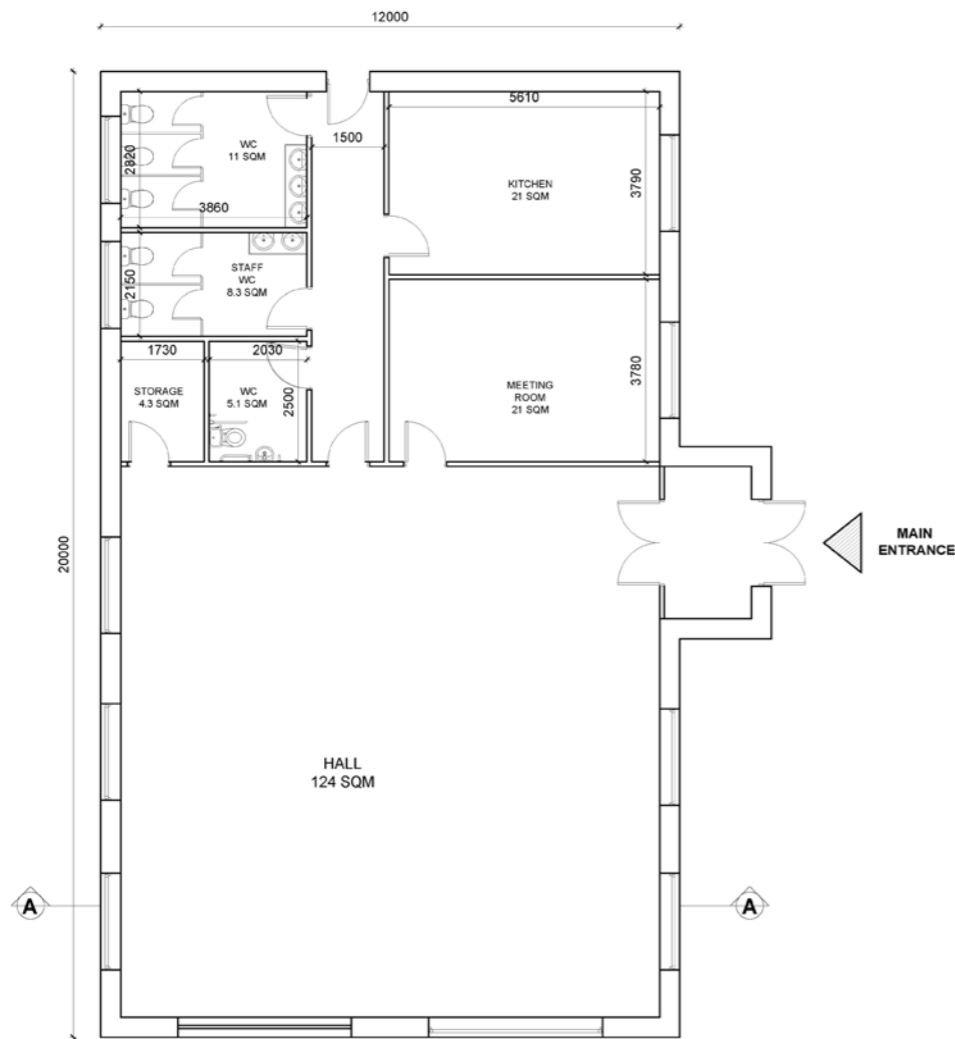


Figure 29. Ground floor plan, front elevation and side entrance to hall/multipurpose indoor space. Source: MCORM, 2022..

2.3.2 SCOUT DEN

A Scout Den is also included in this SHD application to support the wider community and to serve the 360no. unit residential community envisaged and the wider Moygaddy Masterplan future phases. It is located at the Blackhall Little ecological corridor, within the High Amenity Lands, and it is designed to be fully integrated into the sylvan character of the scheme.

This public facility will promote active use of the scheme's blue and green ecological corridors, and the adjoining Public Park to the North.

The building is designed as a simple pavilion to allow for flexible use and boost community, leisure and educational activity within the High Amenity Lands.



Figure 30. Key plan for location of creche. Source: MCORM, 2021..

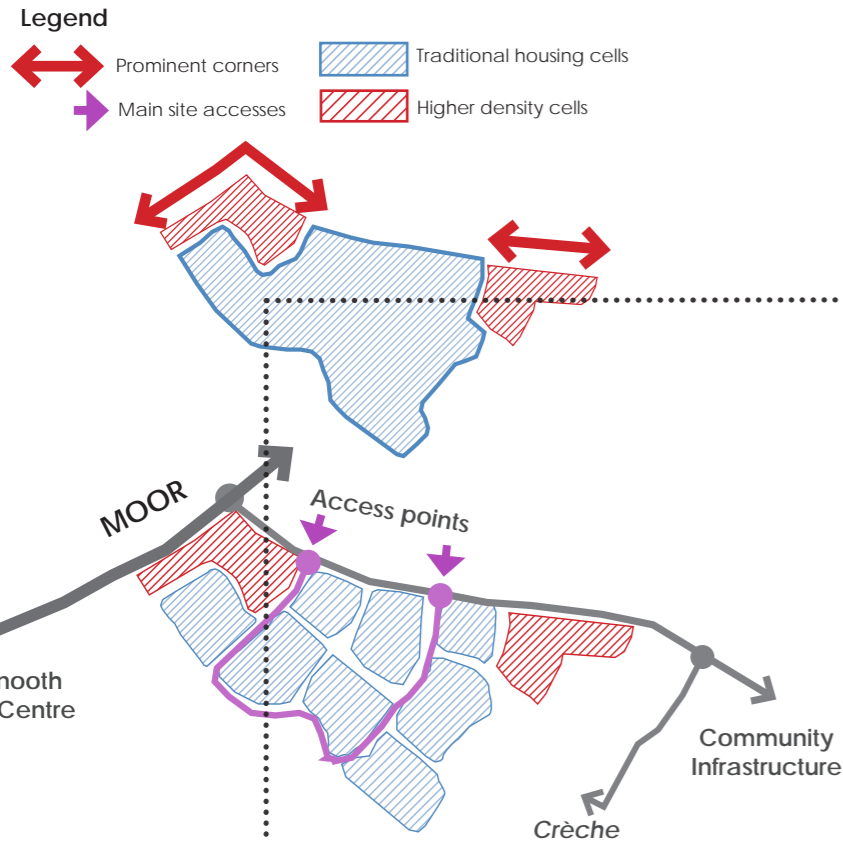


Figure 31. Diagrams of urban design strategy, according to a consistent variety of housing cells framed by higher density precinct of duplex and apartment buildings.. Source: MCORM, 2022.



Figure 33. CGIs of Apartment block A, highlighting corner unit. Source: 3DDB, 2022.



Figure 34. CGIs of duplex buildings with prominent gable facades. Source: 3DDB, 2022



Figure 35. CGIs of red brick houses overlooking the human-scale landscaped open space at the centre of the scheme. Source: 3DDB, 2022.



Figure 32. Building typological variety diagram. Source: MCORM, 2022.

2.4 VARIETY

The presence of two main streets in the form of the MOOR and upgraded L-6219 bounding the subject site along its northern and western edges is an opportunity to locate higher density elements on those frontages. The presentation of the scheme can therefore be marked by more compact areas on the approach to the site from both the east and the west. Typologies associated with higher density urban design solutions will be used to create 3-4 storey built edges along these main streets, with density reducing as you move from the perimeter into the heart of the scheme and facing the riverside park areas.

This concept proposes a diverse but coherent urban design response with a varied mix of residential typologies, combining 4-storey apartment blocks (62 units) and duplexes (102 units), with the location of 2no. 4-storey apartment blocks and 6no. duplex block at the envisaged higher density east and west perimeter cells. These are highlighted in red opposite, with a traditional housing area (196 units) concentrated at the core of the scheme, as illustrated in blue.

The development of this variety concept results in a legible neighbourhood with strong edges acting as markers at principal locations (junction of MOOR and L-6219, and Moyglare bridge), as shown in figures , and attractive housing environment of semi-detached, terraced and wide-frontage typologies alternating two different brick types. Gable entries, along with wide-frontage semi-detached houses (highlighted in yellow) are located at prominent corners which enrich the streetscape variety and contribute to passive surveillance of public realm and parking areas, as illustrated opposite in figure 31.



Figure 36. Sample of proposed elevational variety at buff and red brick houses according to the character area strategy presented in section 2.6. Source: MCORM, 2022.

Variety is also enhanced by a dynamic and inter-connected open space network where small scale pocket parks distributed across the scheme connect with a pedestrian friendly central street network and larger park lands. These will promote healthy movement and deliver a range of leisure outdoor activities, including play areas, a designated Playground adjoining the Public Park and Crèche, and Scout Den. The Crèche and Scout Den will also add variety to the overall built fabric envisaged.

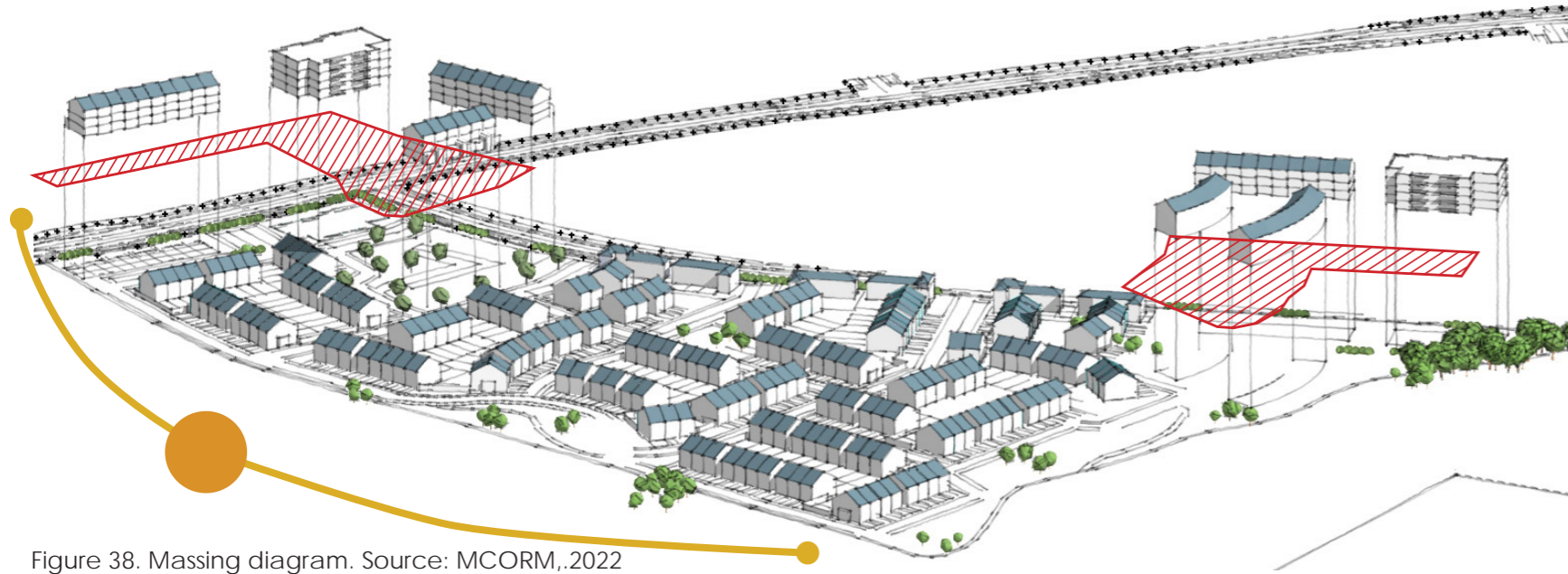


Figure 38. Massing diagram. Source: MCORM, 2022

DEEP PLAN END-TERRACE HOUSE WITH GABLE ENTRY

DEEP PLAN MID-TERRACE HOUSE

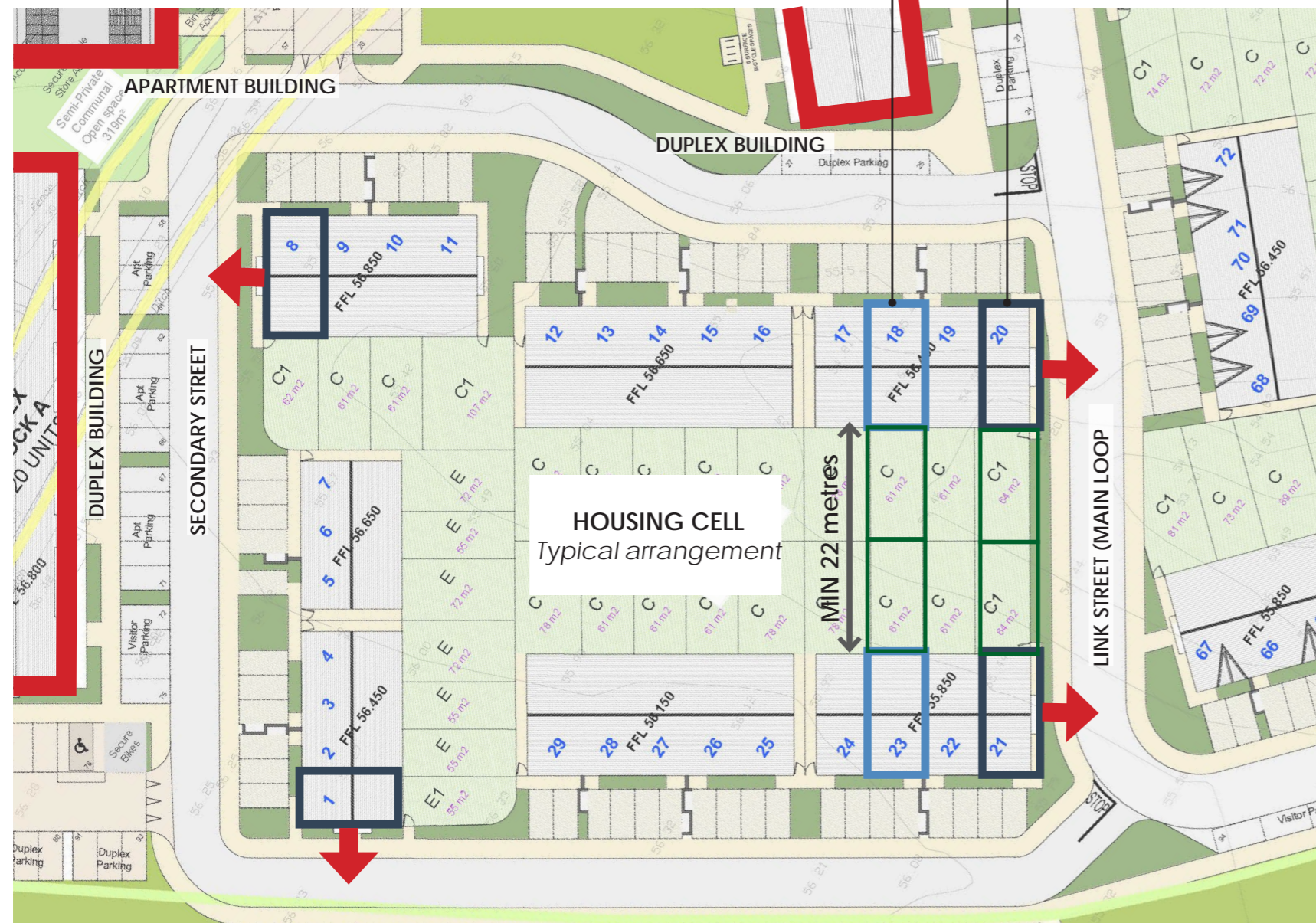


Figure 39. Typical arrangement of Housing cell with efficient deep-plan housing disposition and minimum 22metres distancing back-to-back. Source: MCORM, 2022

2.5 EFFICIENCY

The scheme has been designed to provide an appropriate and efficient use of zoned land as it proposes a number of dwellings (360no. units) at a density of 45.6 units/ha, which is in full accordance with Meath CDP within the zoned residential lands (7.89 HA) included in this application. It is important to note that, despite the gross area of the application site, at 19.52 HA, the residential density is calculated in relation to the residential zoned lands only.

The proposed scheme seeks to promote a balance between efficiency and density through careful design which includes the following:

- A compact urban form that provides higher density multi-unit blocks, which are adjacent to the main services infrastructure (projected MOOR and existing road), which follows the logic of a resources demand for higher density areas.
- The traditional housing area splits into organic but efficient mid-density cells, following the forms of the existing landscape. They will enjoy individual back gardens in compliance with the private open space requirements of the Meath CDP.
- The variety of typologies combining in the scheme delivers a sustainable density of 45.6 units/ha.
- An efficient and DMURS street network, promotes permeability and a pedestrian/cycling friendly public realm.
- Well integrated and distributed public open spaces as part of the overall urban structure, in addition to communal areas adjoining the multi-unit buildings.
- The Crèche and Scout Den buildings are located to the East of the Blackhall Little to create a central community hub and to promote activity in the envisaged parkland at the High Amenity Lands and Public Park.

The table below summarises main site parameters, with particular attention to efficiency and density:

Application Gross Area	19.52 HA
Residential Lands Area	7.89 HA
Total residential units	360 units
Net Density	45.6 units per ha

Figure 37. Efficiency and density brief table. Source: MCORM, 2022

Adequate distancing and orientation of housing cells maximises sunlight/daylight of dwellings. All duplex units are dual aspect and apartment blocks comprise 61% dual aspect units.

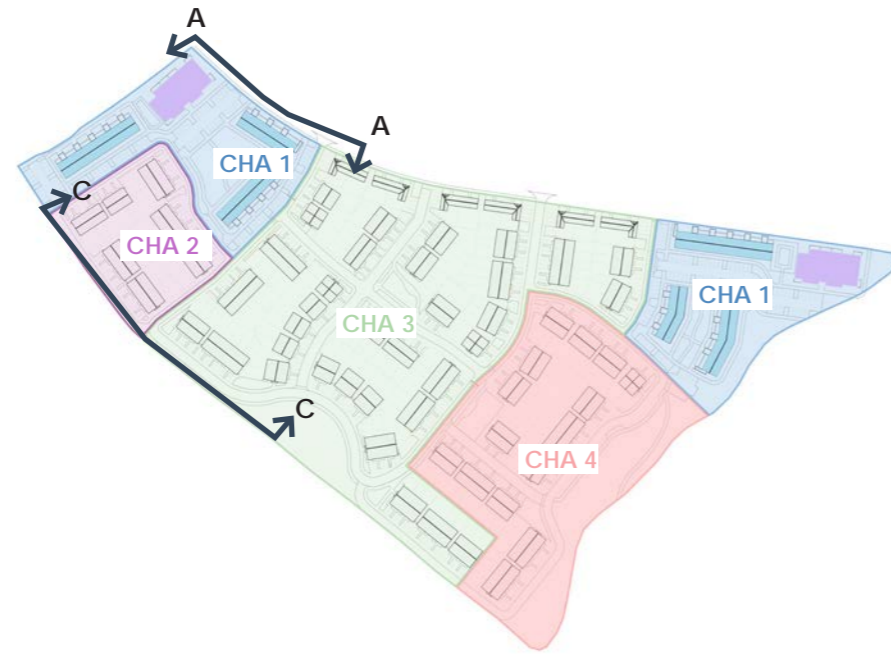


Figure 40. Character Area (CHA) key plan. Source: MCORM, 2022

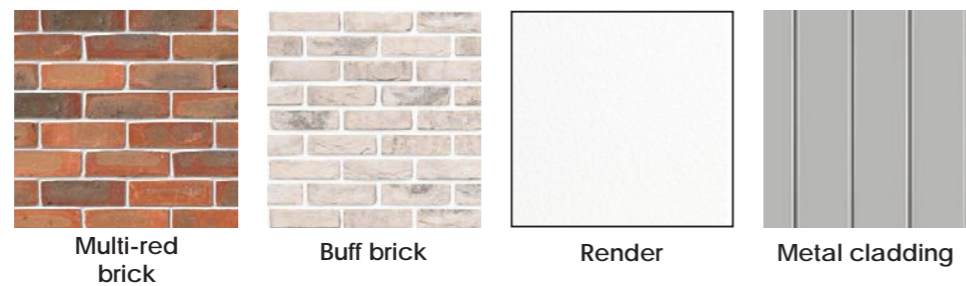


Figure 41. Palette of materials. Source: MCORM, 2022

2.6 DISTINCTIVENESS

A number of distinctive character areas are proposed across the scheme. This strategy will contribute to create a strong sense of place within the envisioned neighbourhood and it will add variety across precincts that will be supported by the use of different external finishes and elevation treatments. These distinct zones will combine with the various housing (semi-detached, terraced, wide frontage units) and multi-unit typologies (apartment blocks and duplexes), resulting in a varied neighbourhood in terms of streetscapes, special corner treatments and a range of building finishes. This will contribute to way-finding throughout the scheme and it will enhance place-making.

The uniqueness of the surrounding landscape has also been considered in this character area strategy. The integration of existing hedgerows and mature woodlands along the River Rye and the Blackhall Little will deliver an attractive neighbourhood, using natural assets in addition to newly landscaped urban parks, including a network of pedestrian and cycle paths.

The palette of materials chosen is shown opposite (fig.41), comprising a combination of buff and red brick throughout the scheme, with render frontages and with same metal cladding detailed on the prominent buildings.

AA



CC



Figure 42. Contiguous elevations showing variety of building types and distinct precincts according to elevation treatments and materials.. Source: MCORM. 2022

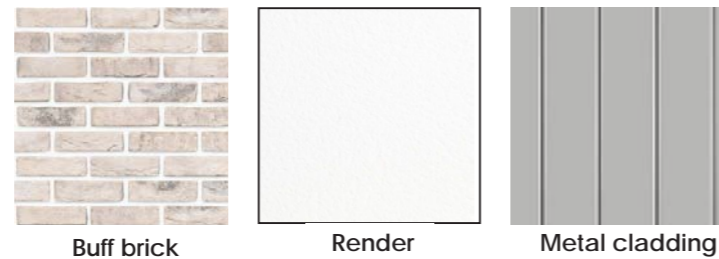
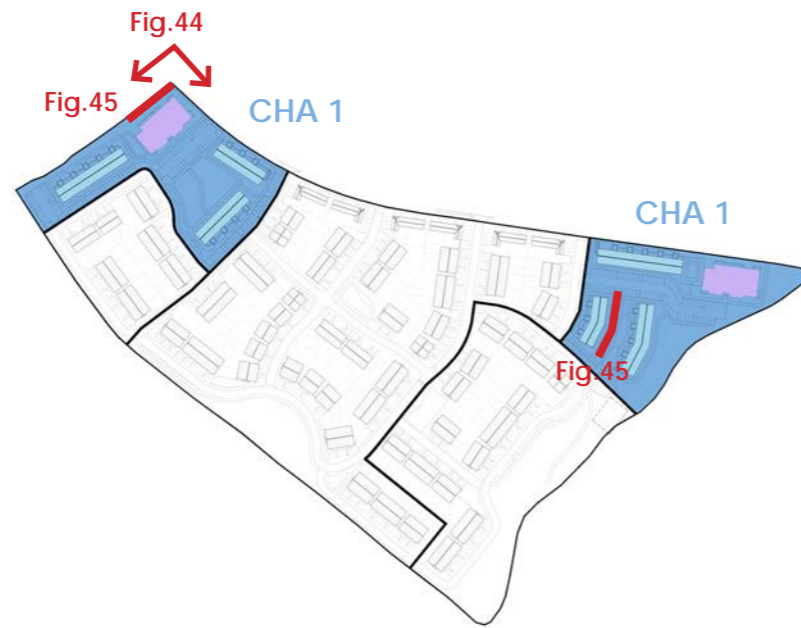


Figure 43. Materials and key plan of character area 2. Source: MCORM, 2022.

2.6.1 CHARACTER AREA 1

Character area 1 is comprised by two areas (marked in blue in the key plan opposite) defined by higher scale buildings in a mix of apartments and duplexes due to its location at the prominent corners of the MOOR (west) and the east-west link (northeast). The use of multi-unit buildings to create strong built frontages also allows for increased density at this appropriate location.

These zones are comprised of 102no. duplexes and 62no. apartments. The duplex blocks include 51no. 1-bed homes and 51no. 2-bed homes. All enjoy a dual aspect arrangement. The two apartment blocks (A&B) located at both north-western and north-eastern prominent corners of the scheme, as highlighted in purple on the key plan provided, comprise 31 apartments, with a mix of 13no. 1-bed and 18no. 2-bed homes on each building. 61% of the apartments are designed with a dual aspect arrangement in both blocks.

Residents of this character area will benefit from a good provision of open space. At the western side, a generous pocket park serves 102no. homes and the adjacent character area. In addition, residents of multi-unit blocks will enjoy designated communal space for each building, physically separated from the public realm by hedgerows and integrated into the overall open space network.

Duplex blocks will be finished in a combination of buff brick and render, as shown in the elevation below, with a ground brick plinth and a recessed upper volume that presents a rhythm achieved by the corresponding to the duplex units. Apartment blocks will be mainly finished in buff brick, with an upper floor in metal cladding adding distinctiveness to this precinct.



Figure 44. CGI showing the strong build edge configured by apartments and duplexes fronting the MOOR and East-West link (road L-6219). Source: 3DDB, 2022.



Figure 45. Sample of elevations of apartment and duplex buildings at character area 1. Source: MCORM, 2022.

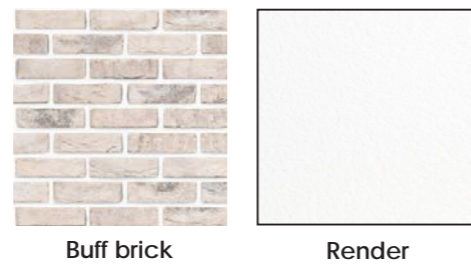
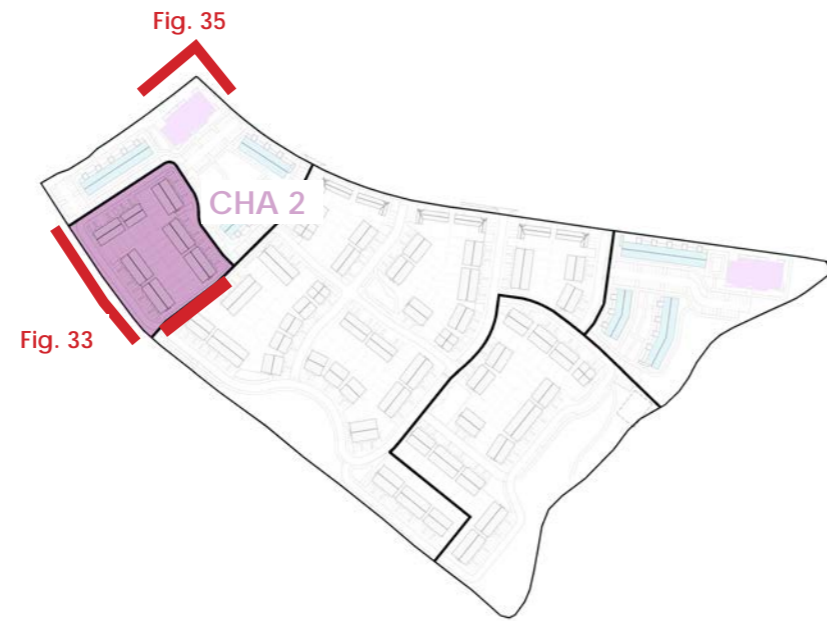


Figure 46. Key Plan and palette of materials. Source: MCORM, 2022.

2.6.2 CHARACTER AREA 2

Located in the southwest area of the scheme adjacent to the River Rye linear park, this precinct is comprised of a well-defined traditional housing cell adjoining character area 1 and also bounded by the loop road, which separates it from the central core of the neighbourhood. The location is indicated in fig.34 opposite.

This area is comprised of 29no. 2-storey terraced and semi detached house typologies, providing 7no. 2-bed and 22no. 3-bed homes. These houses are arranged in efficient layout with 3-bed dwellings in a northeast - southwest orientation generating an attractive frontage to the park facing character area 1. 2-bed units are arranged orthogonally, in a Northwest - Southeast orientation. Dual fronted houses with gable entry are used to ensure good passive surveillance and activation of secondary side streets as illustrated in figures 47 and 48.

Front elevations will be built in buff brick. Rear and side façades will be finished in render. Gable and boundary walls located on prominent corners will also be finished in buff brick to match main frontages and therefore add attractiveness to the street scape at those key points, as displayed below in the side elevation facing the main street of the scheme.



Figure 47. Conceptual aerial view of Character Area 2. Source: MCORM, 2022.



Figure 48. Housing elevational treatments at Character Area 2. Source: MCORM, 2022.

2.6.3 CHARACTER AREA 3

This distinct area constitutes the heart of the scheme and it is contained within the main loop street that provides access to the secondary street network. It is also bounded by the L-6219 road along its northern frontage, which is completed by wide frontage units. This character area also fronts onto the River Rye parkland in the south. It is comprised of 118 houses in total, including 12no. 2-bed, 85no. 3-bed and 21no. 4-bed homes.

All houses are 2-storey. For the most part, they present full brick front elevations with stone detailing around openings. Rear and side elevations are finished in render. The rest of the houses in this area show front elevation treatments in which brick and render combine.

To achieve a safe, active and attractive public realm, end-terrace houses are accessed from the side elevation featuring fully brick finished gable walls. As before, with strong entrance features on the gables, these units ensure the combination of active street frontage throughout the scheme.

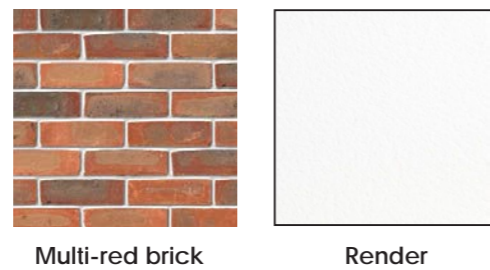
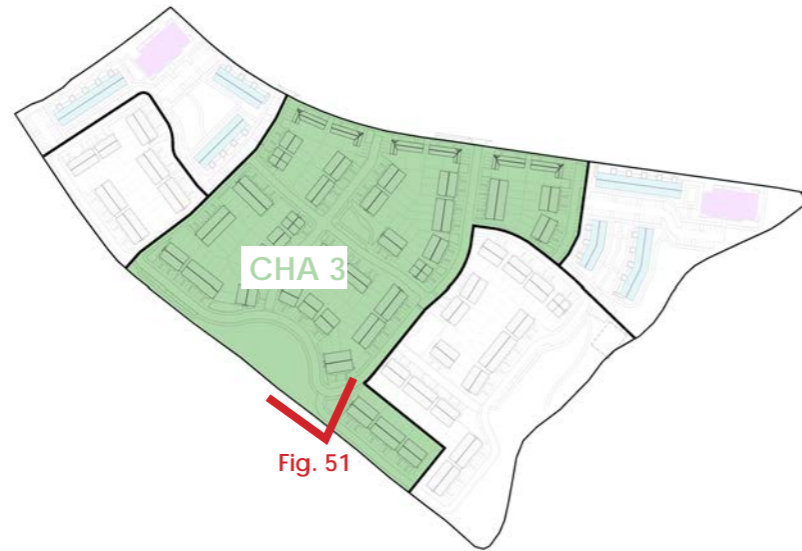


Figure 50. Key plan and palette of materials for character area 3. Source: MCORM.



Figure 49. Character area 2 elevations of semi-detached wide-frontage and terraced deep houses of red-brick housing proposed. Source: MCORM, 2022



Figure 51. CGI of houses overlooking landscaped crescent and southern High Amenity Lands. Source: 3DDB, 2022.



Figure 52. Aerial view of character area 3. Source: MCORM, 2022.

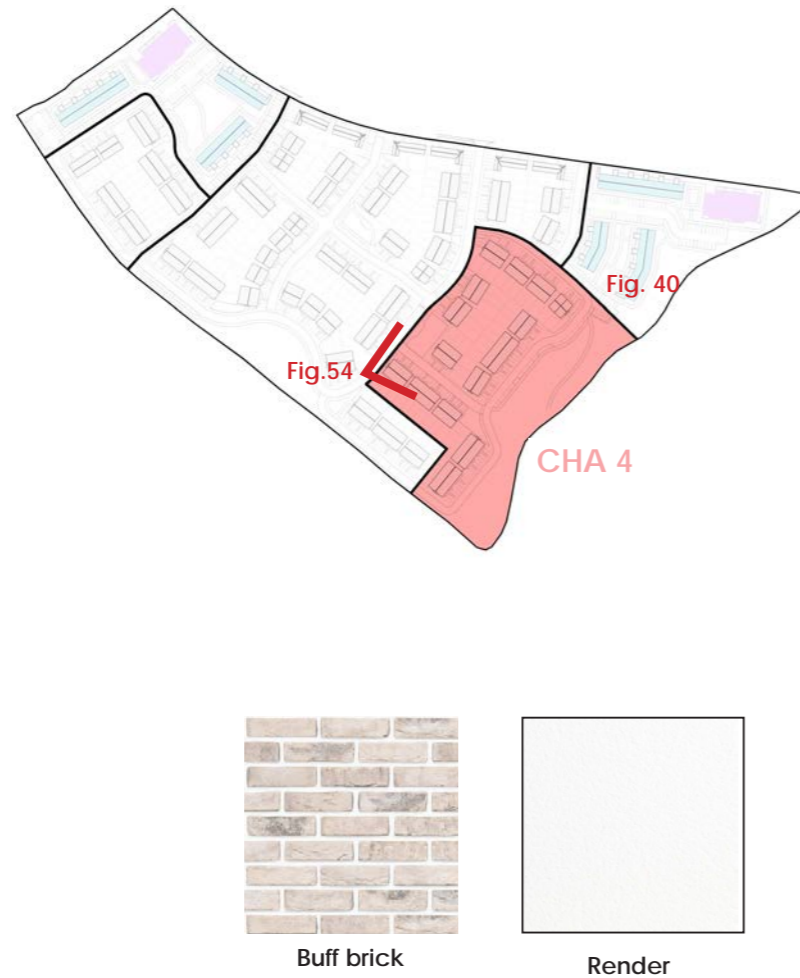


Figure 53. Key plan and palette of materials of character area 4. Source: MCORM, 2021..



Figure 54. CGI of buff brick housing fabric overlooking pocket park landscaped open space. Source: 3DDB, 2022..

2.6.4 CHARACTER AREA 4

This character area is located at the south-eastern portion of the scheme. This cluster of houses benefits on its eastern edge from a full view of the linear park. The units on the western side enclose an intimate pocket park providing opportunities for casual play. This park, along with the other parks across the layout, act as “wayfinders” providing high quality landscape visual breaks between housing cells.

In this regard, this area, along with the duplexes and apartment blocks of character area 1, completes the overall frontage that the scheme presents to the central linear park envisioned in the masterplan framework.

A total of 49no. 3-bed 2-storey houses are located in this area designed in a semi-detached and mid terrace arrangement. They alternate with NW-SE and NE-SW groupings.

Similar to other areas of the scheme, some units are accessed from the gable elevation, which is fully brick-finished, with details in render and a prominent entrance frontage, centrally positioned within the facade.

The artist's impression (fig.42) displayed opposite illustrates the disposition of houses around the pocket park which promotes for active street frontages and passive surveillance.



Figure 55. Character Area elevations. Source: MCORM, 2022..

2.7 LAYOUT

The site layout is based on an efficient arrangement of housing cells with related open spaces in the form of pocket parks which allows easy navigation through a traffic-calmed and permeable residential environment. The predominant 2-storey height skyline is broken by 3-storey duplex and 4-storey apartment buildings, which are strategically located at the north-eastern and north-western edges, as highlighted in red in Figure 56 opposite.

These building markers will act as wayfinders, adding legibility and visual interest to the scheme. Their increased scale provides strong built edges particularly at these corner locations. The northern elevation of the site is composed of wide-frontage houses, along with apartment and duplex buildings at both the MOOR junction and the Blackhall Little bridge. The southern portion of the site, facing the Rye Riverside Park (high amenity lands), is defined by a 3-storey duplex gable facade, carefully designed owing to its prominent location. As you move West to East, the elevational rhythm proposed results in a consistent variety of terraced and semi-detached houses. The latter are configured in an attractive crescent overlooking the southern public open space, marked as C in the diagram. As indicated in dashed line opposite, the southern elevation turns towards the north-east, overlooking the high amenity lands along the Blackhall Little water course, and finishing with a strong prominent corner of 3 and 4-storey buildings at the junction with the bridge over the Blackhall Little. Facing west, the scheme fronts to the MOOR with the same higher-scale volumetric response as you access to the Moygaddy lands through the new planned road.

The existing blue and green natural amenity is fully utilised by transforming these areas into riverside walk parkland that integrates the existing landscaping and hedgerows into the fabric of the new development. In fact, a total of 41% of the site area is proposed as public open space, including both the inner public open spaces and the High Amenity Lands, which connect into the proposed Public Park at the Moygaddy Castle environs, at the north-eastern edge of the residential development. This public park will be a central hub which will ultimately serve the wider masterplan, linking the proposed residential development to future medical, tourism and employment areas. This location will be also activated with the provision of additional public facilities. These facilities include a Crèche, a Scout Den and a Public Playground, envisaged as focal points for both the envisaged 360no. homes' scheme and future developments within the Moygaddy masterplan lands.



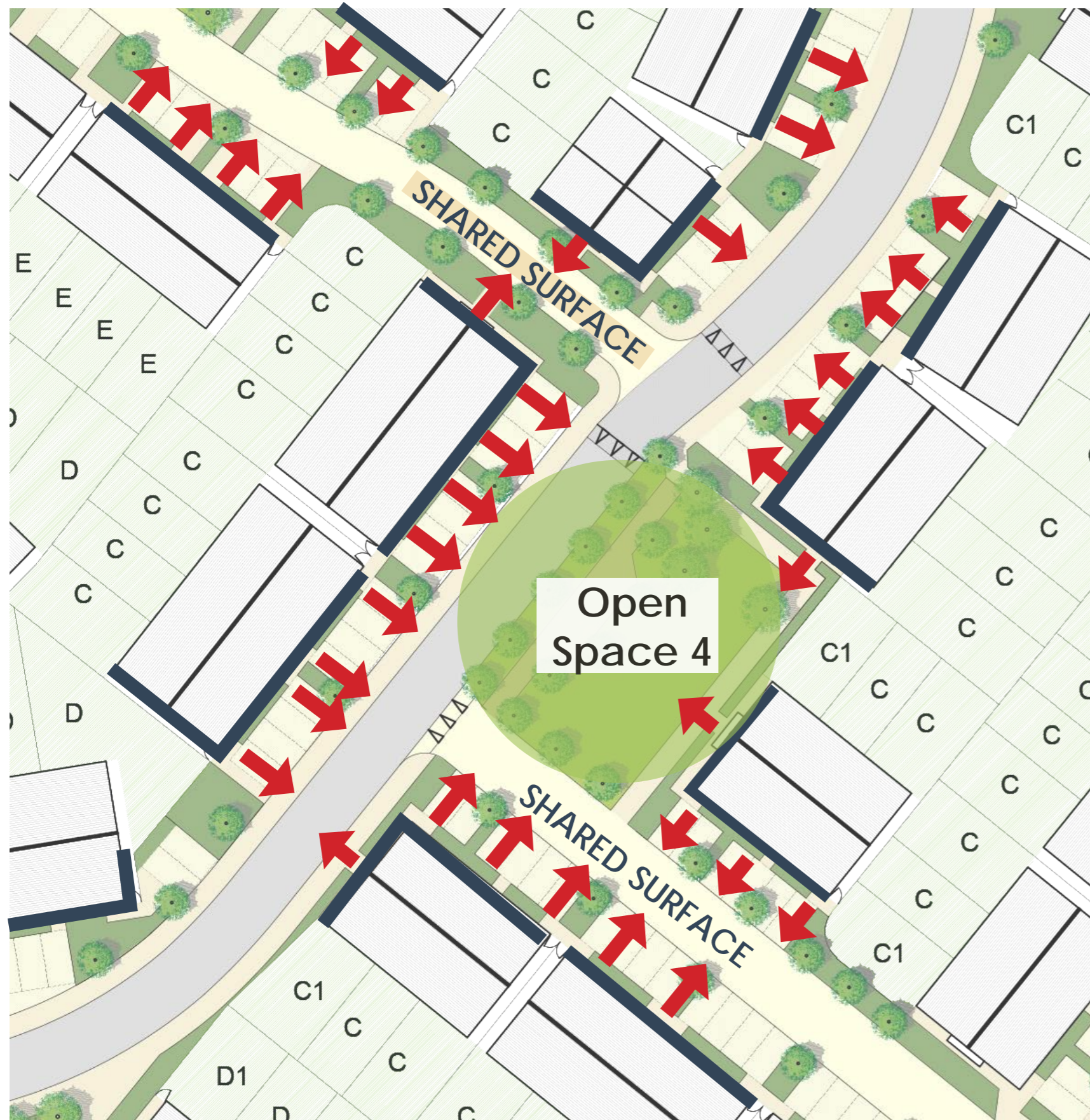


Figure 57. Detail plan of typical pocket park. Source: MCORM, 2022.

As illustrated in Figure 57 opposite, the layout presents a careful and detailed approach to internal elevations and streetscapes. The design brief is to promote permeable, safe and attractive walking and cycling journeys through the scheme, transitioning from the built environment to the green way and blue way amenity. The detail plan adjacent (figure 57) and artist's impression below (figure 58) illustrate the strong relationship between the streets and open spaces proposed. The surrounding residential built fabric is designed to maximise passive surveillance over the public realm, as shown opposite. An arrangement of terraced and semi-detached houses which, in addition to active main streets fronts, include gable entry dwellings accessed from the open space, ensuring adequate overlooking and a high-quality of the streetscape. This approach is also followed at the shared surface junction, which is envisaged as a pedestrian priority public realm area.

The strategy proposed at this location is applied in similar scenarios throughout the scheme, which creates a consistent variety of safe and active public open spaces and streets. In addition, apartment and duplex buildings are organised to generate a minimum of two active frontages to achieve the same result at the communal open space areas of the scheme (please refer to previous overall layout diagram - figure 56).

Legend






-  Loop street
-  Shared surface
-  Built edges
-  Active fronts
-  Open space



Figure 58. CGI of typical pocket park well overlooked by the housing fabric. Source: 3DDB, 2022.

Application Site Gross Area	19.52 HA
High Amenity Lands (H.A.)	6.93 HA
Developable residential area	7.98 HA
Public Open Space (excl. H.A.) - required 10%	0.99 HA (12.6%)
Overall Site Open Space	7.98 HA (41%)

Apartment units	62no. units
Duplex units	102 units
Overall Communal space required	1008 sq.m
Overall Space provided	1202 sq.m

Figure 61. Tables summarising public open space and communal open space provided. Source: MCORM, 2022..

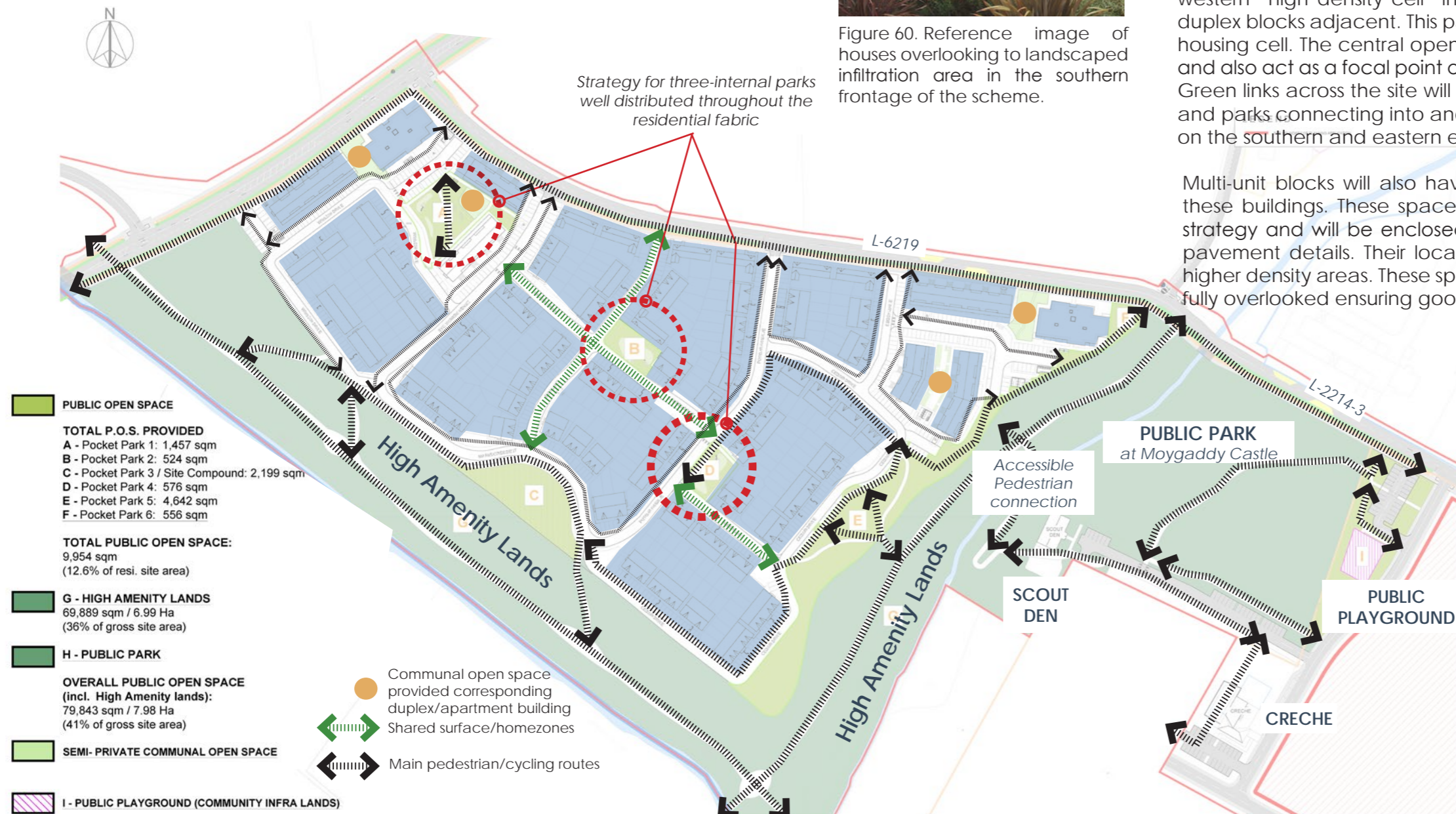


Figure 62. Diagram of public and communal open space provision, including both landscaped areas within the residential lands, high amenity lands and public park. Source: MCORM, 2022.



Figure 59. Reference image of flood resilient park within a rural landscape.



Figure 60. Reference image of houses overlooking to landscaped infiltration area in the southern frontage of the scheme.

2.8 PUBLIC REALM

As 41% of the application gross area is comprised by public open space, the public realm aspect of the scheme has been a key consideration in creating the setting for the new residential neighbourhood. The scheme benefits from an extensive riverside parkland that frames the envisioned neighbourhood along its northern and western edges and provides for generous natural amenities using the existing water courses. This valuable environmental asset will be enhanced by retention and integration of mature woodlands and hedgerows, and the provision of new accessible and permeable routes connecting to the Moygaddy public park, located to the east, all of which will be connected to an internal open space network of intimate pocket parks proposed (indicated in the diagram opposite).






Within the zoned residential lands, the overall open space concept is based on a proportionate distribution of landscaped areas for each of the three zones defined by the main road loop, as highlighted in red in the site layout diagram below. The western "high density cell" includes a pocket park to serve the apartment and duplex blocks adjacent. This park will also be enjoyed by the southern neighbouring housing cell. The central open space will serve the traditional housing cells nearby and also act as a focal point of the scheme and way-finding device in the scheme. Green links across the site will provide an attractive landscaped network of streets and parks connecting into and complimenting the larger parkland areas provided on the southern and eastern edges of the layout.

Multi-unit blocks will also have semi-private communal open spaces relating to these buildings. These spaces will be integrated within the overall open space strategy and will be enclosed and defined by features such as hedgerows and pavement details. Their location responds to the arrangement of the scheme's higher density areas. These spaces, shaded in yellow on the diagram below, will be fully overlooked ensuring good passive surveillance.



Figure 63. Photomontage of Moygaddy Castle Public Park. Source: MCORM, 2022.

Street hierarchy legend

-  MOOR
-  Enhanced East-West link (L-6219 & L-2214-3)
-  Primary internal link street (main loop)
-  Secondary street
-  Communal Parking bay
-  Shared surface street

The internal street network proposed will deliver an efficient and clear hierarchy arrangement through a two-way loop street, which permeates the scheme as highlighted in the diagram below. The overall street layout proposed sets a hierarchical network that establishes the basis for usage, street scape, public realm design and urban form in a clear and legible way.

This hierarchy identifies the loop link street as the main loop and vehicular way-finder through the scheme. Secondary streets access the different neighbourhood character areas. A tertiary level is formed by environmental streets or home zones that will be designed to favour the pedestrian. This hierarchy will also dictate the speed limits, from 30-40km/h for the link street to 20km/h in secondary streets and 10km/h at full pedestrian priority streets.

Within this overall hierarchy traffic is calmed and walking and cycling is prioritised. Parking will be provided for apartment blocks and along secondary streets, on the basis of 1 space per unit + 1 visitor space per 4 units. Houses will be provided with two parking spaces within curtilage. Active frontages will enable natural surveillance of public spaces, surface parking and bike parking.

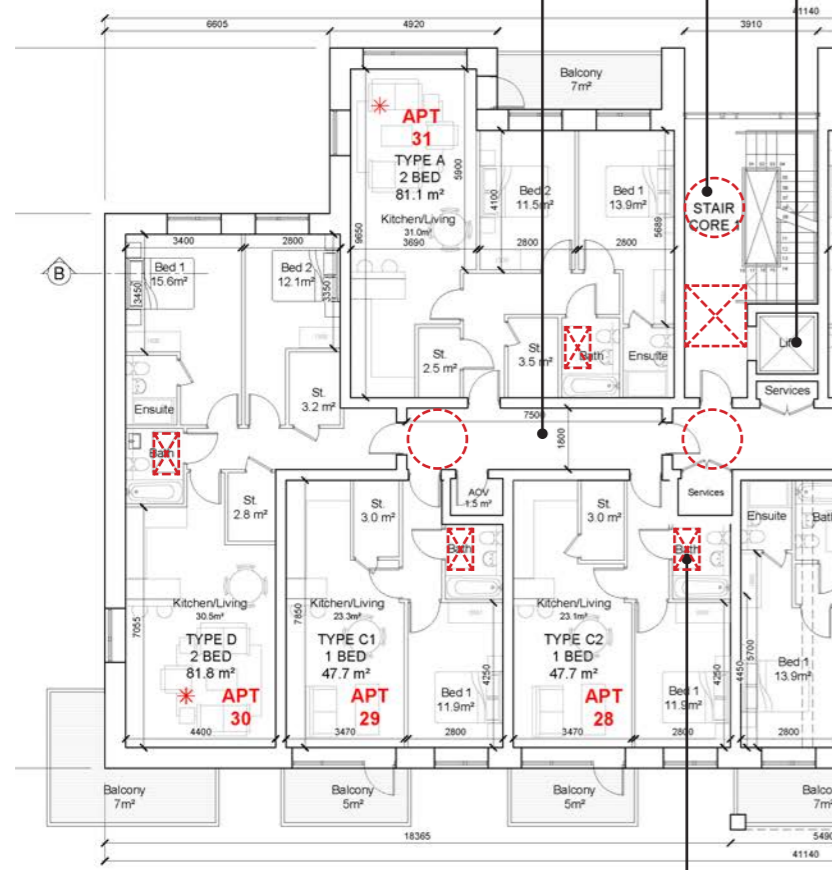
The vehicular traffic routes are conceived on a hierarchical and controlled basis, whilst pedestrian and cycling journeys are envisaged as permeable, both to external locations and within the scheme.



Figure 64. Open space and green link strategy. Source: MCORM

Lift allowing people with a wide range of abilities to travel conveniently and comfortably from floor to floor

Staircase easy to approach, enter and move about in
Communal lobbies designed to a min. width of 1800 for functionality and accessibility



All dwellings are provided with wheelchair accessible bathrooms

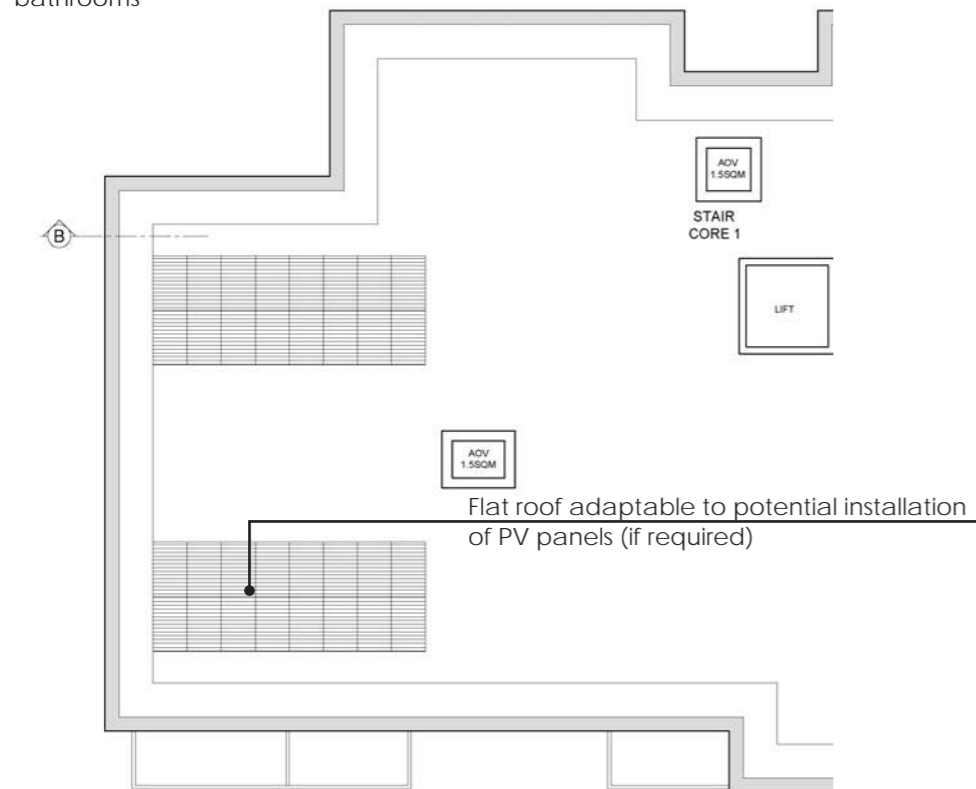


Figure 65. Apartment typical core arrangement and roof plan showing adaptability of living and storage spaces, and potential installation of PV panels, if required, at roof level.

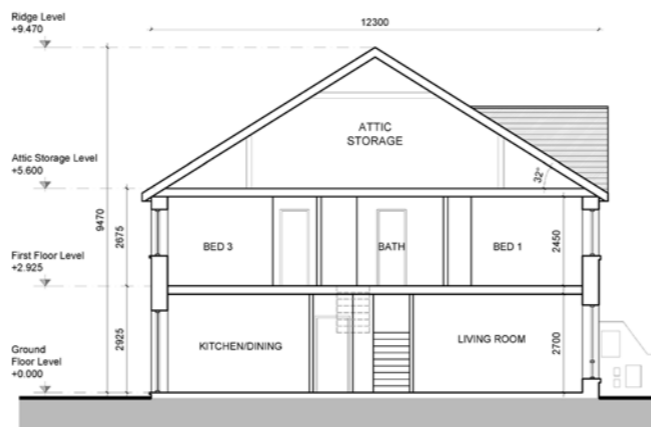
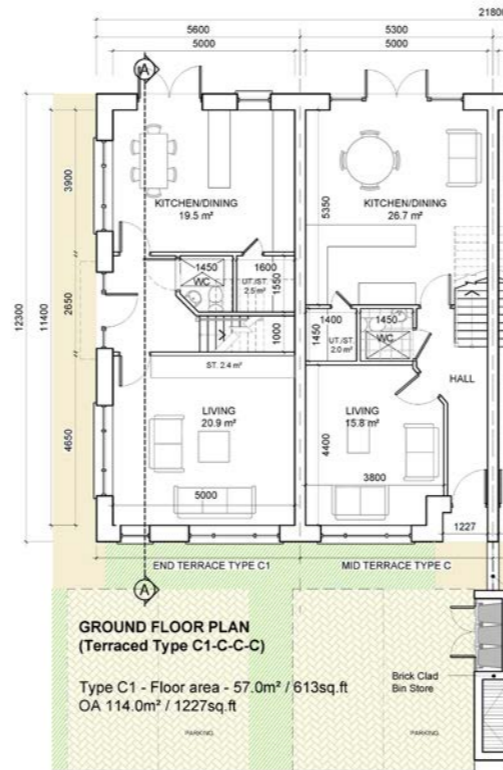


Figure 66. Deep-plan house layouts and section showing adaptability of living and storage areas, including attic.

2.9 ADAPTABILITY

This scheme provides a generous mix of typologies (houses, duplexes and apartments) and dwelling sizes (1 to 4-bed). In this regard, the scheme has had full regard to the following criteria:

- All residential units, from 1 to 4-bed types, have been conceived to successfully cope with change and adapt to future household needs. They are fully designed applying good-practice guidelines and current housing needs and standards.
- All buildings will be built in traditional construction. These buildings will be capable of modification to suit the needs of the end user over time, subject to the necessary statutory consents or adherence to the exempted development regulations.
- Living areas and bedrooms allow for built-in home workstations, if required.
- Internal alterations and/or extensions to the rear or into attic spaces will be possible should occupiers desire to expand their available space in the future.

The range of unit sizes proposed (Fig.67) will allow people to move within the scheme to a larger or smaller dwelling as their needs change over time. Younger occupants may wish to “trade-up” or older residents may wish to “trade-down”. The variety of unit types available will allow people stay within the community for long term.



Figure 67. Key plan showing variety of dwelling typologies. Source: MCORM, 2022



CGI of public open space and communal open space overlooked by duplex blocks



Indicative play area within the landscaped open space network

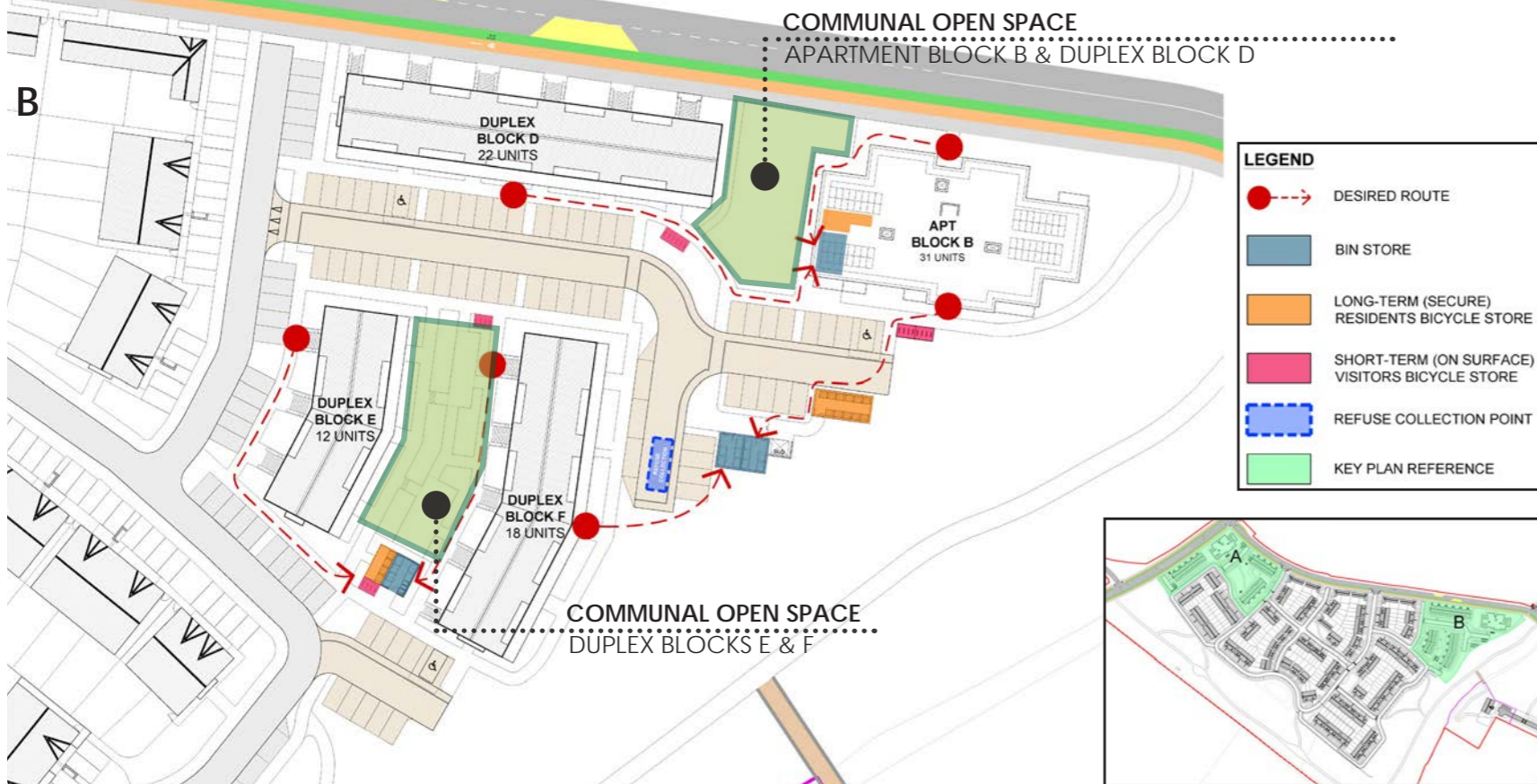


Figure 68. Minimum floor areas for private amenity space. Source: Design Standards for New Apartments, 2020 (Appendix 1).

Figure 69. Key Plan showing locations of communal open spaces provided. Source: MCORM, 2022.

2.10 PRIVACY AND AMENITY

Every residential dwelling proposed in the scheme, regardless of its size or typology, will have an accessible area of private open space to enjoy. Entrances to apartments, duplexes and houses are well-defined with a semi-private transition zone, designed as a soft-landscaped buffer, including hedgerows for privacy where required. Individual terraced areas will be provided for all ground floor units. Similarly, generous terraced areas are provided to serve the duplex dwellings overhead. All private terraces and balconies for apartments and duplexes will comply with the minimum standards outlined in fig. 70 below.

Studio	4 sq m
One bedroom	5 sq m
Two bedrooms (3 person)	6 sq m
Two bedrooms (4 person)	7 sq m
Three bedrooms	9 sq m

Figure 70. Minimum floor areas for private amenity space. Source: Meath County Development Plan, 2021-2027 (Appendix 1)

Apartments and duplexes will also be served by communal open space, well-integrated within the overall landscaped space network, as shown in the site layout details A and B opposite (fig. 68) and dimensioned according to the Design Standards for New Apartments (2020), as follows:

Studio	4 sq m
One bedroom	5 sq m
Two bedrooms (3 person)	6 sq m
Two bedrooms (4 person)	7 sq m
Three bedrooms	9 sq m

Figure 71. Minimum floor areas for communal amenity space. Source: Meath County Development Plan, 2021-2027 (Appendix 1).

In order to provide more attractive and accessible communal areas specifically – with particular reference to the areas between Duplex Block A & Apartment Block A (west of the site) and Duplex Block D & Apartment Block B (east of the site), a ground floor apartment unit has been removed from each apartment block to facilitate the housing of a secure Bins and Bicycles store. This enhances the quality of these communal spaces by removing a single-storey stand alone external store which was previously incorporated at Stage 2. Additional consideration has also been given to the shared communal areas between Duplex Block B & C and also Duplex Block E & F which have been enhanced for Stage 3, as informed in section 1.4 An Bord Pleanála Response of this document.

All houses have generous private rear gardens, which are compliant in size and design, as per the standards outlined in Meath County Development Plan (2021-2027):

House Type	Minimum Area of Private Open Space to be Provided
One/two bedroom	55sq.m.
Three Bedroom	60sq.m.
Four bedrooms or more	75sq.m.

Figure 72. Minimum floor areas for private amenity space. Source: Design Standards for New Apartments, 2020 (Appendix 1).

Moygaddy Castle STAGE 3
21006

Date: August 2022



HOUSING QUALITY ASSESSMENT

UNIT TYPE	AREA (M ²)	QUANTITY					TOTAL	% DUAL ASPECT
		CRECHE	1 BED (2 Person)	2 BED (4 Person)	3 BED (5 Person)	4 BED (6 Person)		
APARTMENTS								
BLOCK A	2,124		13	18			31	19 of 31
BLOCK B	2,124		13	18			31	19 of 31
TOTAL	4,248		26	36	0	0	62	61%
DUPLEX								
BLOCK A	1,424		10	10			20	20 of 20
BLOCK B	995		7	7			14	14 of 14
BLOCK C	1,139		8	8			16	16 of 16
BLOCK D	1,563		11	11			22	22 of 22
BLOCK E	952		6	6			12	12 of 12
BLOCK F	1,324		9	9			18	18 of 18
TOTAL	7,397		51	51	0	0	102	100%
HOUSES	22,328		0	19	156	21	196	
TOTAL RESIDENTIAL UNITS	33,974		77	106	156	21	360	
			21%	29%	43%	6%	100%	%
CRECHE	514	1						
SCOUT DEN	220							
OVERALL TOTAL	34,707							

PROJECT STATISTICS			
OVERALL SITE AREA		195,223 sq.m	19.52 ha
DEVELOPABLE SITE AREA			7.89 ha
DENSITY			45.6 units per ha
*note developable site area equals overall site area less access road & High Amenity Lands			
HIGH AMENITY LANDS			6.99 ha
PLOT RATIO - (Residential lands)			0.40
PLOT RATIO - (Residential & High Amenity Lands)			0.18
SITE COVERAGE GROSS (incl. creche, Scout Den, external Bin Stores, external Bike stores & ESB Sub stations)			10%
10% REQUIRED POS			0.79 ha
OPEN SPACE PROVIDED			0.99 ha (12.5%)
COMMUNAL OPEN SPACE			
		REQUIRED	PROVIDED
DUPLEX BLOCK A & APT BLOCK A	120 + 198sqm =	318sqm	319sqm
DUPLEX BLOCK B & C	84 + 96sqm =	180sqm	181sqm
DUPLEX BLOCK D & APT BLOCK B	132 + 198sqm =	330sqm	371sqm
DUPLEX BLOCK E & F	72 + 108sqm =	180sqm	331sqm
TOTAL		1008sqm	1202sqm
CRECHE PROVISION			
Note: Creche based on provision for Phase 1A Lands (i.e. 185 houses (20 (4-bed) + 155 (3-bed) + 10 (50% 2-bed)) + 26 (50% 2-bed duplex) + 19 (50% 2-bed apartments) = 230* units with 230 / 75 x 20 child spaces = Creche required for 61 children			
*1-Bed Apartments & 50% of 2-Bed Apartments Discounted in Creche Unit Calculations			

Please refer to architect's HOA document for full information of residential mix and creche provided, and related calculations for open space, storage and parking provisioning, in addition to areas of rooms, private terraces and gardens.

2.11 PARKING

Car parking has been incorporated into a well landscaped streetscape, both in terms of paving finishes and integration within the overall scheme layout. Grouped communal parking has been provided for communally for the apartment and duplex units and. On-curtilage parking has been provided for the houses. Standards outlined below have been followed for the parking provision:

- 2 parking spaces per house at the front of each individual entrance door and bike storage in rear gardens.
- 1 resident parking space per unit and 1 visitor space each 4 units for apartment and duplex blocks.

Bike parking has been provided according to 1no. space for each apartment and duplex unit within indoor safe stores. In addition, 1no. space per 2 units is provided for visitors in outdoor sheltered areas, adjacent to each building, either at the ground floor level of apartment blocks A and B, or at designated external bikes and bins storage areas serving duplex blocks C, D, E and F. Please refer to section 2.12 for additional information on both bike and bin secure storage.

COMMUNAL CAR PARKING (DUPLEXES)

COMMUNAL CAR PARKING (APARTMENTS)

ROAD CARRIAGEWAY (DMURS COMPLIANT)

ACCESSIBLE PEDESTRIAN FOOTPATH (PART M COMPLIANT)

OWN-CURTILAGE CAR PARKING (HOUSES)

REAR GARDENS (SEE SECTION 2.10 PRIVACY AND AMENITY)

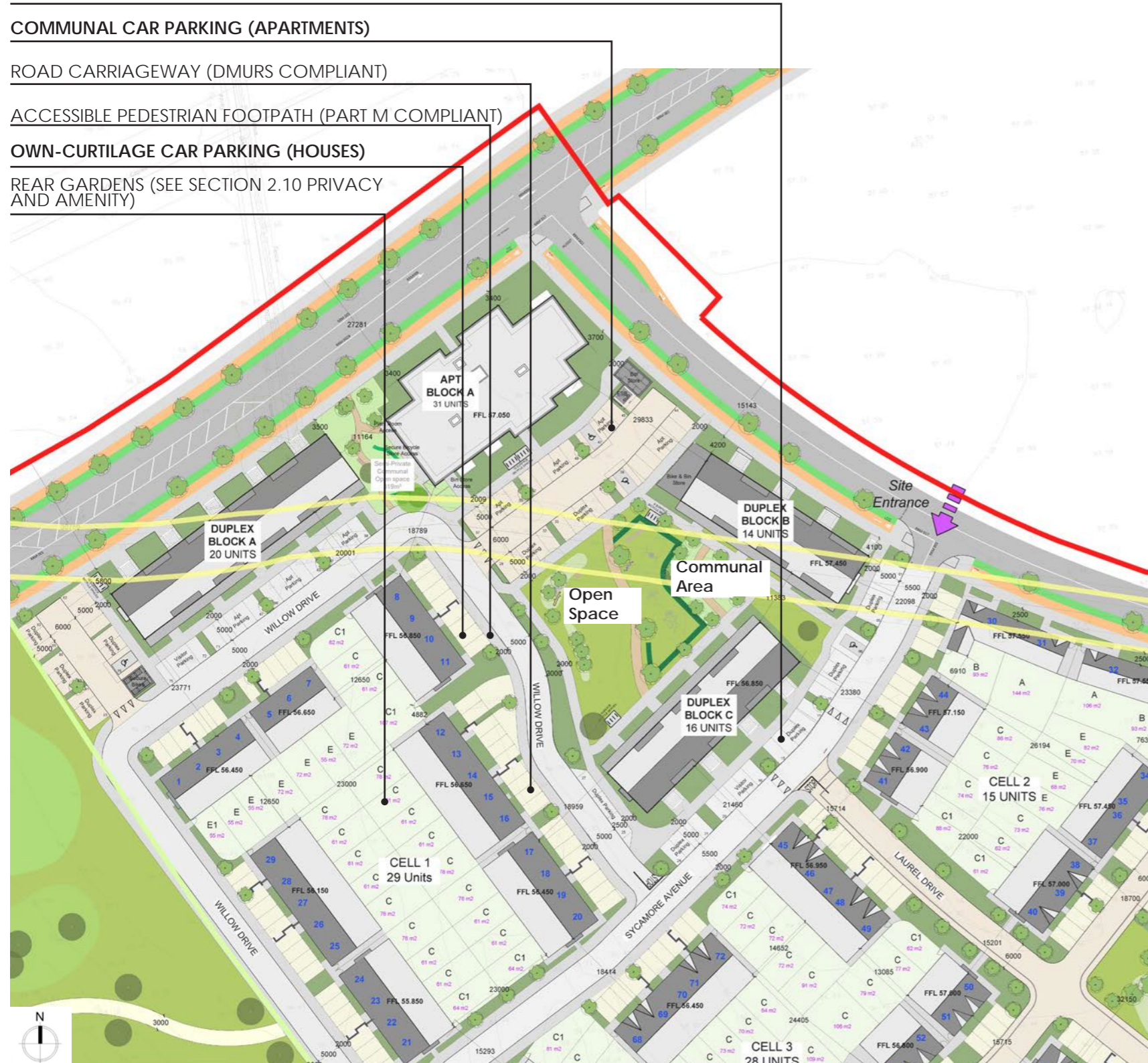


Figure 73. Detail of parking provision integrated in the proposed overall urban design strategy, featuring own curtilage of houses and communal areas adjacent to duplexes and apartments, including accessible spaces. Source: MCORM, 2022.

HOUSES	3 & 4 Bed Houses - 2 spaces per unit 2 Bed Houses - 1.5 spaces per unit
DUPLEXES	BLOCK A - F 1.25 spaces per unit 128 Surface Parking Spaces (including 4 no. universally accessible or 3%)
APARTMENT BLOCKS	BLOCK A + B 1.25 spaces per unit 78 Surface Parking Spaces (including 3 no. universally accessible or 4%)
VISITOR	17 Surface Parking Spaces Note: 5% Provision For Electric Vehicle Allowed
UNIVERSALLY ACCESSIBLE	7 Surface Parking Spaces

Figure 73

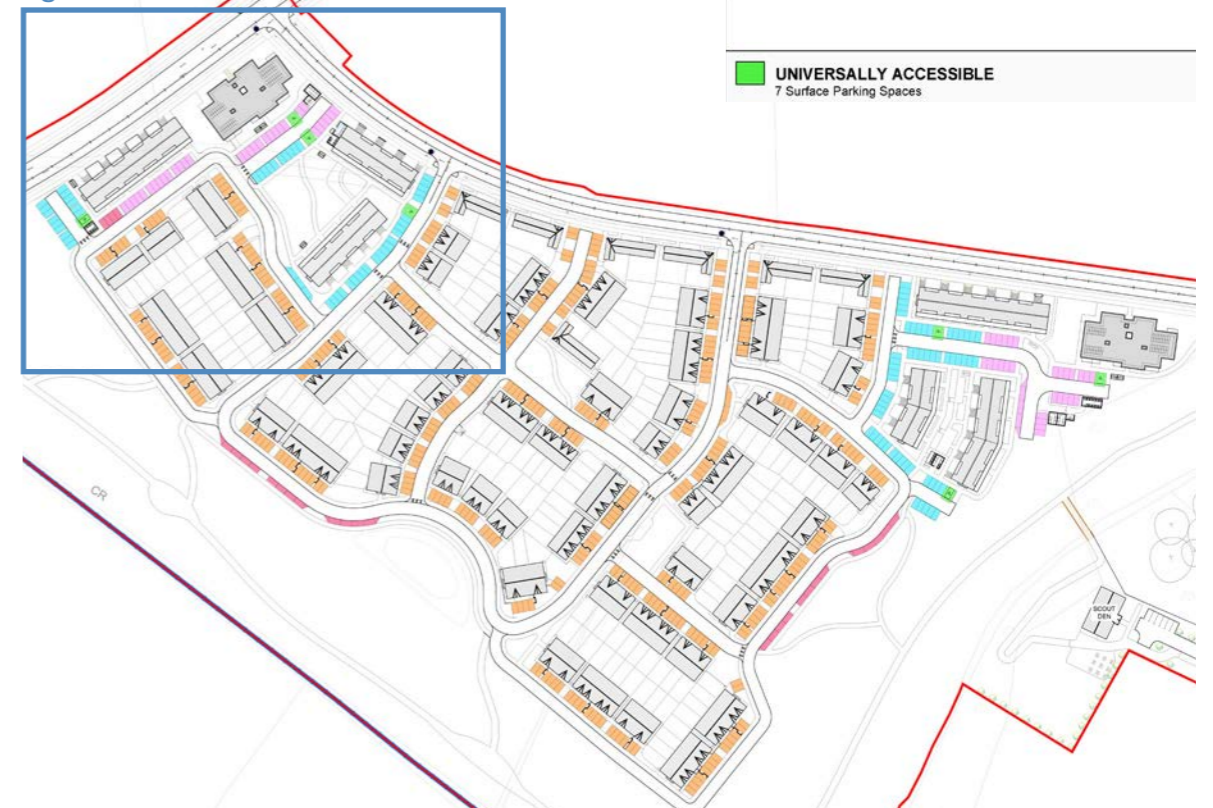


Figure 74. Residential parking layout and legend specifying parking location and usage: house residents, apartment/duplexes residents and visitors. Source: MCORM, 2022.

CAR PARKING

BICYCLE PARKING

APARTMENT BLOCK A (31 UNITS)			APARTMENT BLOCK B (31 UNITS)			DUPLEX BLOCK A (20 UNITS)			DUPLEX BLOCK B & C (30 UNITS)			DUPLEX BLOCK D (22 UNITS)			DUPLEX BLOCK E & F (30 UNITS)																				
Total Spaces (1.25 per Apartment Unit):			38.75	(39 surface)	Total (1 per Unit + 1 visitor for every 2no. units)			47	(31 secure + 16 surface)	Total Spaces (1.25 per Apartment Unit):			38.75	(39 surface)	Total (1 per Unit + 1 visitor for every 2no. units)			47	(31 secure + 16 surface)																
includes:			Disabled *	1	(includes 1 no. Surface Level EV Space)	includes:			Resident	31	(secure)	includes:			Disabled	1	(includes 1 no. Surface Level EV Space)	includes:			Resident	32	(secure)												
			Visitor *	8	(of which 1 no. EV spaces provided)				Visitor	16	(surface parking)				Visitor	8	(of which 1 no. EV spaces provided)				Visitor	16	(surface parking)												
Total Spaces (1.25 per Apartment Unit):			25	(25 surface)	Total (1 per Unit + 1 visitor for every 2no. units)			30	(20 secure + 10 surface)	Total Spaces (1.25 per Apartment Unit):			38	(38 surface)	Total (1 per Unit + 1 visitor for every 2no. units)			45	(30 secure + 15 surface)	Total Spaces (1.25 per Apartment Unit):			28	(28 surface)	Total (1 per Unit + 1 visitor for every 2no. units)			33	(22 secure + 11 surface)						
includes:			Disabled	1	(includes 1 no. Surface Level EV Space)	includes:			Resident	20	(secure)	includes:			Disabled	1	(includes 1 no. Surface Level EV Space)	includes:			Resident	30	(secure)	includes:			Disabled	1	(includes 1 no. Surface Level EV Space)	includes:			Resident	22	(secure)
			Visitor	5	(of which 1 no. EV spaces provided)				Visitor	10	(surface parking)				Visitor	8	(of which 1 no. EV spaces provided)				Visitor	15	(surface parking)				Visitor	8	(of which 1 no. EV spaces provided)				Visitor	11	(surface parking)
Total Spaces (1.25 per Apartment Unit):			38	(38 surface)	Total (1 per Unit + 1 visitor for every 2no. units)			45	(30 secure + 15 surface)	Total Spaces (1.25 per Apartment Unit):			38	(38 surface)	Total (1 per Unit + 1 visitor for every 2no. units)			45	(30 secure + 15 surface)	Total Spaces (1.25 per Apartment Unit):			38	(38 surface)	Total (1 per Unit + 1 visitor for every 2no. units)			45	(30 secure + 15 surface)						
includes:			Disabled	1	(includes 1 no. Surface Level EV Space)	includes:			Resident	30	(secure)	includes:			Disabled	1	(includes 1 no. Surface Level EV Space)	includes:			Resident	30	(secure)	includes:			Disabled	1	(includes 1 no. Surface Level EV Space)	includes:			Resident	30	(secure)
			Visitor	8	(of which 1 no. EV spaces provided)				Visitor	15	(surface parking)				Visitor	8	(of which 1 no. EV spaces provided)				Visitor	15	(surface parking)				Visitor	8	(of which 1 no. EV spaces provided)				Visitor	15	(surface parking)

* Provision of facilities for the charging of battery operated cars at a rate of up to 5% of the total car parking spaces. All House parking spaces will be constructed to be capable of accomodating future charging points, as required.

Car Parking

Bicycle Parking

CRECHE			SCOUT DEN			PUBLIC PARK																							
Total Spaces			29	(29 surface)	Total (1 per 1 Staff member)			12	(12 surface)	Total Spaces *			6	(6 surface)	Total Spaces			12	(12 surface)	Total Spaces*			24	(24 surface)	Total Spaces			12	(12surface)
includes:			Setdown/Drop off	5	includes:					includes:					includes:					includes:					includes:				

* An additional 18 spaces have been provided for dual-use parking spaces for Public Park & future expansion of Creche. Please refer to the site plan drawing for the location of these spaces.

Please refer to architect's HQA document for full information of parking provision for the proposed residential amenity and facilities provided within this SHD application.



Figure 75. Palette of materials and correspondent CGIs of the four distinct areas proposed. Source: MCORM, 2022.

2.12 DETAILED DESIGN

The detailed design of the scheme has been carefully considered as part of the design development process. This section addresses the external architectural material character and detailing of the residential fabric proposed, along with the selection of hard landscaping finishes including integrated car parking and to positioning of bins/bikes storage within the public realm.

A consistent design of details and finishes for the built fabric and public realm, following a 4 character area strategy and having due regard to the resulting streetscape and open space provision.

High quality buff and red brick elevations align with current developments that are being built in the area (north County Kildare). This combination of finishes will be durable and ensure the quality of the scheme stands the test of time.

An aesthetic variety will be achieved by using a combination of brick and render throughout the different character areas envisaged. The quality and variety of finished proposed is illustrated in the CGI's opposite (Fig. 75) which are linked to the character area plan below.



Figure 76. Key plan of the 4 character areas proposed Source: MCORM, 2022.

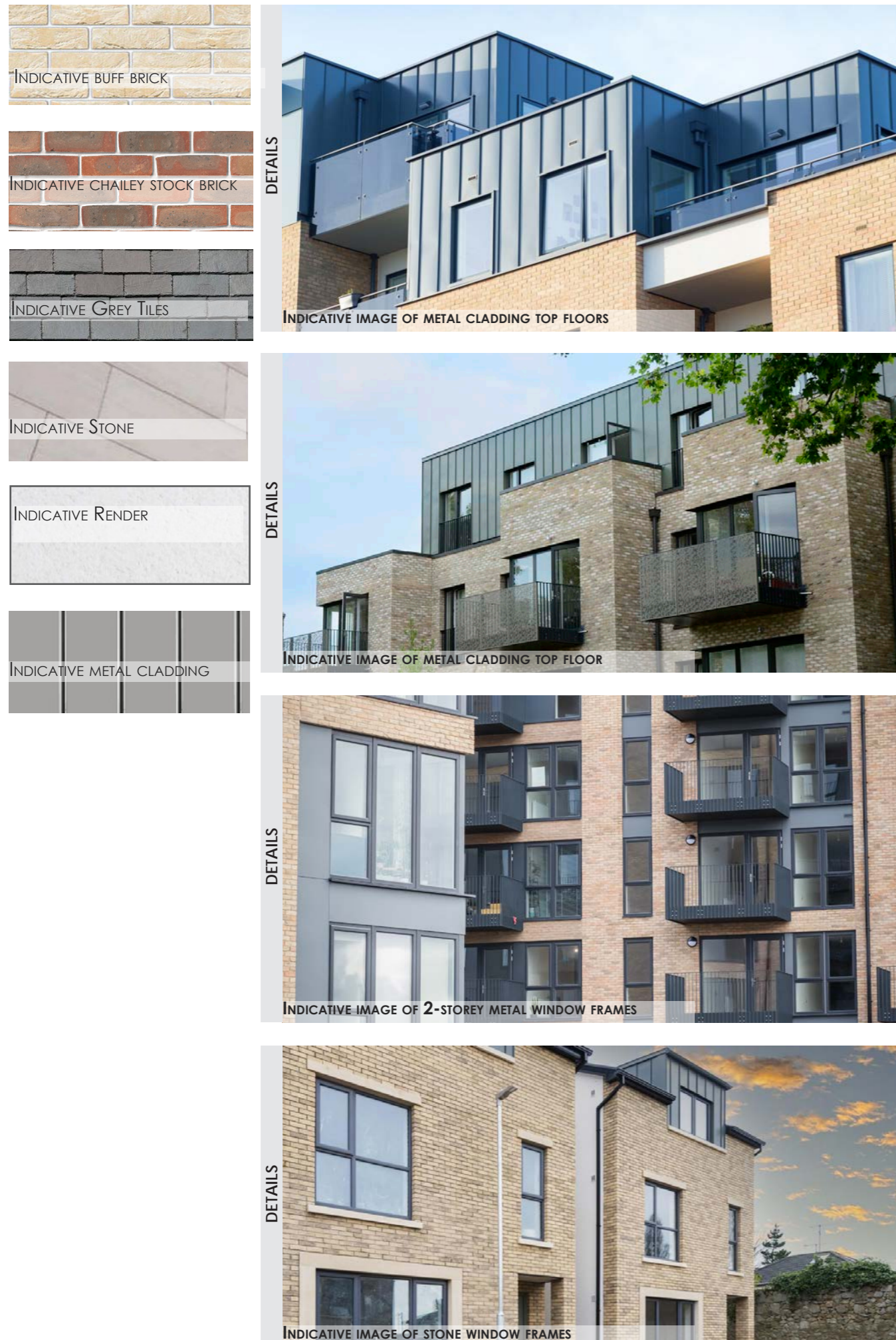


Figure 77. Indicative images for reference on architectural detailed design. Source: MCORM, 2022.

Recessed apartment block penthouses will be finished in metal cladding, providing both strong distinctive contrast from the overall brick envelope and durable protection against the weather.

Duplexes present specific window frame features. This consists in recessed metal panels that connect first floor and second floor windows. In addition, gable fronts in prominent locations present specific treatments in which buff brick and render infill combine with detailed design openings at first and second floor, in addition to ground floor entrances (blocks C,D, E and F) to achieve a positive image and adequate passive surveillance of the surrounding public realm. Please refer to figure 78 below.



Figure 78. Proposed design for gable elevations of duplex buildings showing prominent entrance to ground floor units and windows of duplex unit ensuring aesthetic streetscapes and passive surveillance of public realm and communal areas adjacent. Source: MCORM, 2022.

Houses will incorporate high quality stone elements framing openings at all character areas, as shown in lower reference image. While stronger frames surround the openings at ground floor level, windows heads and sills are delineated by lighter elements at first floor levels.

A selection of hard landscaping materials according to the proposed street configuration and integration of the different instances that take place within the public realm and the transitional spaces to private areas has been chosen. As illustrated in section the across a secondary street, all housing units will enjoy landscaped buffers and on-curtilage outdoor car parking areas rendered fully as an integral element of the streetscape. These will be finished in permeable paving to maximise both the street quality and rainfall infiltration, and will also minimise maintenance requirements.



Figure 79. Secondary road cross-section exemplifying horizontal arrangements and materialisation of streets in the scheme. Source: MCORM, 2022.

AREA A



AREA B



LEGEND

- - - - - -> DESIRED ROUTE
- BIN STORE
- LONG-TERM (SECURE) RESIDENTS BICYCLE STORE
- SHORT-TERM (ON SURFACE) VISITORS BICYCLE STORE
- REFUSE COLLECTION POINT
- KEY PLAN REFERENCE



Figure 80. Areas of multi-unit buildings in the scheme showing detail on communal services provided for bin and bike storage. Source: MCORM, 2022.

Figure 81. Key plan locating detail areas A and B provided. Source: MCORM, 2022.

Storage for bins and bikes has been carefully considered within the overall urban design strategy in order to provide accessible and usable facilities to encourage sustainable transport modes.

Houses will have bin storage within their own curtilage arranged in between car parking spaces and integrated within the soft landscaping proposed for these areas between the public realm and each home. We respectfully state that all housing units enjoy enough indoor storage and rear garden areas to store bikes safely and comfortably.

Multi-unit blocks will include communal and secured bin and bike storage either at ground floor level indoor areas of apartment blocks A and B or at external sheltered storage spaces adjacent to the duplex blocks (C,D,E & F) to which they serve.

These storage areas are calculated according to tables provided below, taking into consideration the number of units that will use each of them. Their location is selected according to residents' minimised desired routes across accessible and inclusive public realm and communal areas, and efficient refuse collection points for waste and recyclables, which are envisaged to be accessible from waste management trucks driving throughout the proposed street network at apartment and duplex precincts, highlighted in green on the key plan provided opposite (areas A and B).

In particular, residents will enjoy separated long-term secured storage, highlighted in orange in drawings provided (1no. bike per unit) while short-term on surface bike parking facilities are provided for visitors, indicated in red opposite, (0.5no. bikes per unit).

The detail design of these service elements is developed to integrate them within the streetscapes and landscaped areas of the scheme, applying high-quality finishes including a combination of ventilation metal louvres and brick cladding, maximised for houses and for prominent locations within apartment and duplex areas, and steel louvres for adequate ventilation.

Please refer to drawing PL 114 for further information regarding the layout of storage elements envisaged and to drawings 700-705 for detail on the same.

AREA A		AREA B	
BIN & BIKE STORAGE CALCULATIONS		BIN & BIKE STORAGE CALCULATIONS	
DUPLEX BLOCK A & APT BLOCK A (51 UNITS)		DUPLEX BLOCK D & APT BLOCK B (53 UNITS)	
BIN STORAGE	BICYCLE STORAGE	BIN STORAGE	BIKE STORAGE
- 15 X 1100 L	- 51 RESIDENTS	- 15 X 1100 L	- 53 RESIDENTS
- 4 X 360L	- 26 VISITORS	- 4 X 360L	- 27 VISITORS
DUPLEX BLOCK B & DUPLEX BLOCK C (30 UNITS)		DUPLEX BLOCK E & DUPLEX BLOCK F (30 UNITS)	
BIN STORAGE	BICYCLE STORAGE	BIN STORAGE	BIKE STORAGE
- 7 X 1100 L	- 30 RESIDENTS	- 10 X 1100 L	- 30 RESIDENTS
- 3 X 360L	- 15 VISITORS	-	- 15 VISITORS
TOTAL BIN STORAGE	TOTAL BICYCLE STORAGE	TOTAL BIN STORAGE	TOTAL BIKE STORAGE
- 22 X 1100 L	- 122	- 25 X 1100 L	- 125
- 7 X 360L		- 14 X 360L	

Figure 82. Bin and bike storage calculations for adequate provision of the same. Source: MCORM, 2022.

3 CONCLUSION



The proposed design for the Moygaddy Castle SHD has addressed the unique characteristics of the site and its local context within the Maynooth Environs. Distinct character areas including a variety of residential typologies and a careful integration of existing natural site features, such as the River Rye and Black Hall Little water courses, along with diverse mature woodlands and hedgerows, integrate a 360no. units residential community that will enjoy visual interest in built form and an inclusive mix of unit types and sizes for a wide range of households.

The site layout has been designed to integrate well into the natural landscape and takes advantage of the High Amenity Lands opportunity, providing ecological buffers which bound the scheme along the Rye River and Blackhall Little waterways.

The housing fabric proposed will set the standard for future residential applications within the Moygaddy Masterplan. This SHD application will also deliver key infrastructure to access the scheme, including the adjoining section of the planned MOOR and the enhancement of the L-6219 and L-2214-3 roads, which cross the Moygaddy lands from west to east. In addition, the residential community envisaged will be served by a Crèche, a Scout Den, a Public Park at the historic Moygaddy Castle tower and a Public Playground, which will contribute to generate a strong sense of place and community that will benefit from a range of activities within the envisaged public realm and parklands. These public facilities will be located adjoining the central green spine of the Moygaddy Masterplan - zoned as High Amenity - to be accessed by the proposed 360no. residential estate and future developments within the wider masterplan area.

The scheme presents a strong urban design response to the site, designing a traffic-calmed environment of efficient 2-storey housing cells which is framed by higher-density cells at the north-western and north-eastern corners. These are prominent locations in which 4-storey apartment buildings and 3-storey duplexes are proposed as legible building markers overlooking the MOOR junction with the L-6219 and the High Amenity lands at the Moyglare bridge. The variation in scale at this particular location, including apartment and duplex blocks along with 2-storey housing typologies, creates variety within an overall coherent built environment.

Permeability through the site has been reinforced by a naturally traffic-calmed street network and a number of pedestrian/cycle routes bounding the site along the existing water courses, which ensures that the neighbourhood remains well connected to the wider Moygaddy Masterplan towards the north and east, and to Maynooth Town Centre to the south. Future permeability and connections to adjacent lands have been provided, which will allow shorter pedestrian and cycle journey times to Maynooth, as envisaged in the masterplan framework and Meath CDP (2021-2027)

The proposed neighbourhood will bring a legible urban structure of housing cells, strong streetscape elements, high quality materials and landscaped areas used throughout the site, which will result in a positive residential development for Moygaddy.

We refer An Bord Pleanála to the architectural drawings that accompany the application for further detail of the architectural design and site layout proposed. We respectfully suggest that layout results in a residential scheme that is well connected and integrated with its unique natural amenity and surroundings, and delivers a high-quality and sustainable neighbourhood with a unique sense of place for its end-users and greater community at Moygaddy.

4 CGI VISUALS





A. CGI showing strong frontage of apartments and duplex buildings to the MOOR and junction with the enhanced L-6219. Source: 3DDB, 2022



B. CGI illustrating landscaped open space overlooked by 3-storey duplex buildings. Source: 3DDB, 2022



C. CGI illustrating landscaped open space and central homezone area overlooked by red brick houses in character area 3. Source: 3DDB, 2022



D. CGI illustrating landscaped crescent adjacent to southern parkland, overlooked by semi detached red brick houses. Source: 3DDB, 2022



E. CGI of pocket park overlooked by buff brick houses at character area 4. Source: 3DDB, 2022



F. CGI showing duplex buildings in streetscape of visual interest featuring front and gable aesthetic elevations. Source: 3DDB, 2022